



CITY OF LAKEWOOD
PLANNING AND ENVIRONMENT COMMISSION
REVISED REGULAR MEETING AGENDA

Thursday, May 1, 2025 – 7:00 p.m.

**City Council Chambers, The Centre at Sycamore Plaza
5000 Clark Avenue
Lakewood, California 90712**

How to Submit Public Comment:

Members of the public who wish to submit public comment may do so using one of the following methods. We ask that you please indicate the specific item on which you wish to be heard or whether you wish to make a public comment on a matter that is not on the agenda but is within the subject matter jurisdiction of the Planning and Environment Commission.

- 1. In-Person:** Speakers are asked to complete the Sign-In Sheet located at the entrance of the Council Chambers. Speakers shall be limited to three (3) minutes per person per item unless modified by the Chair.
- 2. Email:** Public comments may be emailed to ckojaku@lakewoodca.gov. Public comments submitted electronically by 4:00 p.m. on the meeting day will be provided to the Planning and Environment Commission before or at the meeting but will not be read during the meeting. Comments submitted after 4:00 p.m. will be forwarded to the Commission but may not be received before the meeting.

Persons with Disabilities. Any person with a disability who requires a modification or accommodation in order to participate in a meeting should direct such a request to the Community Development Department by emailing ckojaku@lakewoodca.gov or calling the Department at (562) 866-9771, extension 2303 at least 48 hours before the meeting, if possible, to ensure that reasonable arrangements can be made to provide accessibility to the meeting.

REVISED PLANNING AND ENVIRONMENT COMMISSION AGENDA

May 1, 2025

- **CALL TO ORDER**
- **PLEDGE OF ALLEGIANCE**
- **ROLL CALL:**
 - Chairperson Stuckey
 - Vice-Chairperson Cole
 - Commissioner Baca
 - Commissioner Garcia-Salas
 - Commissioner Rowland

1. APPROVAL OF MINUTES

- A. Minutes from the April 3, 2025 Planning and Environment Commission

Recommended Action: Approval

2. ANNOUNCEMENTS AND PRESENTATIONS

The Community Development Director or his designee may address the Commission on matters of general information and/or concern.

3. PUBLIC HEARINGS

A. 5809 LAKEWOOD BOULEVARD CONDITIONAL USE PERMIT NO. 1026 - APPLICANT: DUTCH BROS (BRIANNA UY FOR BARGHAUSEN)

The proposed project is a Conditional Use Permit (No. 1026) to allow for a drive-thru restaurant known as Dutch Bros. The project site is zoned C-4 (General Commercial) and has a Land Use designation of Commercial.

Environmental Determination: The City may determine a project to be exempt from CEQA pursuant to Section 15061.(b). This project is Categorically Exempt from the California Environmental Quality Act, based on Sections 15301.(1).(3) and 15303.(c) of the CEQA Guidelines.

Recommended Action: Adopt a Resolution of the Planning and Environment Commission of the City of Lakewood approving Conditional Use Permit Case No. 1026 for a drive-thru restaurant located at 5809 Lakewood Boulevard, subject to the recommended conditions of approval.

REVISED PLANNING AND ENVIRONMENT COMMISSION AGENDA

May 1, 2025

4. REPORTS

A. 5228 LAKEWOOD BOULEVARD CONDITIONAL USE PERMIT NO. 1013 – 1-YEAR REVIEW APPLICANT: SAVERS

Recommended Action: Receive and file.

5. PUBLIC COMMENTS

This is the time set aside for members of the public to address the Planning and Environment Commission on items of interest that are not on the agenda, but are within the subject matter jurisdiction of the Planning Commission. Pursuant to the Brown Act, the Planning and Environment Commission cannot answer any questions or take any action on a matter that is not on the agenda.

6. ADJOURNMENT

To the regular meeting of the Planning and Environment Commission on Thursday, June 5, 2025 at 7:00 p.m. at The Centre at Sycamore Plaza, 5000 Clark Avenue.

DIVIDER SHEET

**CITY OF LAKEWOOD
PLANNING AND ENVIRONMENT COMMISSION
MEETING OF APRIL 3, 2025
MINUTES**

CALL TO ORDER

A regular meeting of the Lakewood Planning and Environment Commission ("Commission" or "PEC") was called to order by Chairperson Stuckey at 7:00 p.m. in the City Council Chambers, 5000 Clark Avenue, Lakewood, California.

PLEDGE OF ALLEGIANCE

Commissioner Garcia-Salas led the Pledge of Allegiance.

ROLL CALL: *Present:* Chairperson Stuckey
 Commissioner Garcia-Salas
 Commissioner Rowland
 Excused: Vice Chairperson Cole
 Commissioner Baca

STAFF & CONSULTANTS:

Aldo Cervantes, Director of Community Development
J. Patrick McGuckian, Assistant Director of Community Development
Paul Kuykendall, Senior Planner
Carolyn Lehouillier, Housing Specialist
Josh Yordt, Director of Public Safety
Kaitlin Sanchez, Public Safety Manager
Shireen Kulkarni of Risk Management Professionals
Ivy Tsai, City Attorney
Cindy Kojaku, Administrative Secretary

APPROVAL OF MINUTES

There being no objections, Chairperson Stuckey ordered the Minutes of the Regular Meeting of March 6, 2025, approved as submitted.

ANNOUNCEMENTS AND PRESENTATIONS

There were none.

PEC PUBLIC HEARINGS:

1. Conditional Use Permit No. 1026, 5809 Lakewood Boulevard, Request Approval for a Drive-Thru Restaurant – DUTCH BROS (Applicant: Brianna Uy for Barghausen).

Recommendation: Open the public hearing and continue the item to the May 1, 2025 meeting.

Senior Planner, Paul Kuykendall, (Senior Planner Kuykendall) presented that the applicant requested continuance of their matter to the next regular Planning and Environment Commission Meeting on May 1, 2025.

Chairperson Stuckey opened the public hearing.

Chairperson Stuckey asked if there is anyone in the audience who would like to speak on this item?

There were none.

Commissioner Stuckey kept the public hearing open.

Commissioner Rowland moved and Commissioner Garcia-Salas seconded approval of staff recommendation to continue the Conditional Use Permit No. 1026, 5809 Lakewood Boulevard, and its related categorical exemptions, to the next regular Planning and Environment Commission Meeting on May 1, 2025.

Chairperson Stuckey said there is a motion and a second and called for a roll call vote.

AYES: COMMISSIONERS: Rowland, Garcia-Salas, Stuckey
NOES: COMMISSIONERS: n/a
ABSENT: COMMISSIONERS: Baca, Cole
ABSTAIN: COMMISSIONERS: n/a

2. CDBG Five-Year Consolidated Plan for Fiscal Year 2025-2029, One Year Action Plan for Fiscal Year 2025-2026, and Analysis of Impediments to Fair Housing Choice

Recommendation: Open the public hearing for citizen input for the proposed Fiscal Year 2025-2029 Consolidated Plan, Fiscal Year 2025-2026 Action Plan, and the Analysis of Impediments to Fair Housing Choice; and following the public hearing, move to direct staff to take into consideration all comments received by the public and recommend approval of the proposed Consolidated Plan, Action Plan, and Analysis of Impediments to Fair Housing Choice by the City Council on April 22, 2025.

Housing Specialist, Carolyn Lehouillier, (Housing Specialist Lehouillier) presented the staff report and exhibits which recommended approval of the CDBG Five-Year Consolidated Plan for Fiscal Year 2025-2029, One Year Action Plan for Fiscal Year 2025-2026, and Analysis of Impediments to Fair Housing Choice.

These documents are on file with the Community Development Department. The Notice of Hearing was properly delivered and posted pursuant to the Lakewood Municipal Code and state law. Staff recommends that the Commission hold a public hearing and, following the hearing, move to direct staff to take into consideration all comments received by the public and recommend approval of the proposed Consolidated Plan, Action Plan, and Analysis of Impediments to Fair Housing Choice by the City Council on April 22, 2025. Housing Specialist Lehouillier asked if there are any questions of staff.

Chairperson Stuckey asked if there are any questions of staff.

There were none.

Chairperson Stuckey opened the public hearing.

Chairperson Stuckey asked if there is anyone in the audience who would like to speak on this item?

There were none.

There being no one else wishing to be heard on the matter, Commissioner Stuckey closed the public hearing.

Commissioner Rowland moved and Chairperson Stuckey seconded approval of staff recommendation to recommend the City Council to approve the proposed Consolidated Plan, Action Plan, and Analysis of Impediments to Fair Housing Choice by the City Council on April 22, 2025.

Chairperson Stuckey said there is a motion and a second and called for a roll call vote.

AYES:	COMMISSIONERS: Rowland, Garcia-Salas, Stuckey
NOES:	COMMISSIONERS: n/a
ABSENT:	COMMISSIONERS: Baca, Cole
ABSTAIN:	COMMISSIONERS: n/a

REPORTS:

1. Local Hazard Mitigation Plan Workshop

Public Safety Manager, Kaitlin Sanchez, introduced Shireen Kulkarni (Representative Kulkarni) of Risk Management Professionals, Inc. who presented the oral report.

Representative Kulkarni presented the oral report summarizing the written staff report and slide presentation.

Representative Kulkarni recommended that this agenda item does not require action by the PEC, rather the purpose of the LHMP workshop is to provide an opportunity for the PEC to provide feedback on the draft LHMP.

Chairperson Stuckey asked if there are any questions of staff.

There were none.

Chairperson Stuckey thanked Representative Kulkarni. Chairperson Stuckey stated she participated in the creation and review of this Plan, it was very intensive, and she was able to learn a lot more about the City.

Chairperson Stuckey asked if anyone in the audience would like to speak.

There were none.

PUBLIC COMMENTS:

None.

STAFF COMMUNICATIONS:

None.

ADJOURNMENT: The meeting was adjourned at 7:26 p.m.

Next meeting will be May 1, 2025.

Secretary

DIVIDER SHEET



**CITY OF LAKEWOOD
PLANNING AND ENVIRONMENT COMMISSION**

Meeting Date: May 1, 2025

From: Paul Kuykendall, AICP, Senior Planner 
Aldo Cervantes, Director of Community Development 

Subject: Conditional Use Permit No. 1026

Location: 5809 Lakewood Boulevard

Proposed Use: Drive-Thru Restaurant

Applicant: Brianna Uy, Barghausen, representing Dutch Bros

SUMMARY

The applicant, Brianna Uy, Barghausen, representing Dutch Bros, is requesting approval for Conditional Use Permit (CUP) No. 1026 to establish a drive-thru restaurant (a coffee shop) which will replace an existing restaurant (formerly Pancho's Heritage restaurant) located at 5809 Lakewood Boulevard. Staff recommends the Planning and Environment Commission (PEC) hold a public hearing, and following the hearing, move to adopt the attached Resolution approving Conditional Use Permit No. 1026, subject to findings and conditions contained therein and approve the related Categorical Exemption (Categorically Exempt from the California Environmental Quality Act (CEQA) based on Sections 15301.(l).(3) and 15303.(c) of the CEQA Guidelines).

INTRODUCTION

The proposed drive-thru restaurant will replace the existing restaurant building along with a reconfiguration of the site to accommodate the proposed drive-thru restaurant. The site is designated by the General Plan as Commercial and is zoned C-3 (Intermediate Commercial). The request is being made pursuant to Lakewood Municipal Code Section 9347.D.6 which allows drive-thru restaurants in the C-3 zone provided that a CUP has been obtained and remains in full force and effect.

STATEMENT OF FACTS

Area Description

The site is located at the northwest corner of Lakewood Boulevard and South Street. The following table lists the land uses and zoning designations for the site and the surrounding properties (see Vicinity Map and Aerial View).

AREA	EXISTING LAND USE	ZONE DESIGNATION
Subject Site	Commercial (vacant restaurant)	C-3 (Intermediate Commercial)
North	Commercial (Walgreens drug store)	C-3 (Intermediate Commercial)
East	Commercial (Mobil gas station, LL Flooring, PPG Paints)	C-3 (Intermediate Commercial)
South	Jack in the Box drive-thru, South Plaza commercial center	C-3 (Intermediate Commercial)
West	North Plaza commercial center	C-3 (Intermediate Commercial)

Site Description

The 37,902 square-foot site is developed with a 5,845 square-foot restaurant building. The Walgreens to the north is on a separate parcel, but there is a reciprocal agreement where access and parking for 117 parking spaces is shared between the site and Walgreens. The site is accessed by two (2) driveways along Lakewood Boulevard, one (1) driveway from South Street, and a public alley to the north by way of the reciprocal agreement. The six-foot tall, six-foot, three-inch wide monument sign located next to the westerly property line adjacent to a driveway at Lakewood Boulevard will be removed.

Project Description

The project involves demolition of the existing 5,845 square-foot restaurant building and trash enclosure to prepare the site. A new one-story 940 square-foot drive-thru building will be built in approximately the same location as the existing restaurant. The floor plan consists of a primary production area, drive-thru and walk-up pick-up windows, a mechanical room, restroom, and electrical room (see Exhibits “A,” “B,” “C,” and “D”). A 300 square-foot canopy will be located over an outdoor patio at the west side of the building. The patio will have eight seats. The proposed building has no interior dining areas. Metal awnings will be located above the drive-up window, entry and service doors, and the east elevation.

Dual drive-thru lanes will be constructed around the building with traffic flowing in a counter-clockwise direction. The drive-thru lanes merge into a single lane past the menu board but before the pick-up window. Approximately 410 linear feet of stacking space is available behind the drive-through window with a queue capacity for approximately 19 vehicles. The southerly driveway at Lakewood Boulevard will be reduced in width to create an exit-only driveway to prevent vehicle traffic from backing into the public right-of-way. The drive-through will not include any speaker boxes, rather all customer orders are taken in person either, at the window or with a runner that carries a handheld device to transmit orders to the kitchen. This minimizes noise impacts and decreases the amount of vehicle idling at menu boards. In the event that a speaker box is installed at a future date, the proposed Resolution contains a condition that all drive-thru menu boards and speakers shall have adjustable volume controls so that sound generated by the drive-thru will not be audible at the closest property line.

The application states that Dutch Bros “will implement a runner system at the proposed facility that is designed to increase speed and efficiency in serving drive-through customers. Dutch Bros Coffee employees travel from vehicle to vehicle to greet customers and take orders. These “runners” utilize a handheld device to transmit customers’ orders to the multiple drink stations

inside the building. Additionally, runners will charge individuals while in line, so by the time they arrive at the service window they may pick up their order and be on their way. This system decreases wait times, while allowing the runners to have a more personal face-to-face interaction with customers.”

A new trash enclosure will be constructed along the north side of the site, between Dutch Bros and Walgreens (see Exhibits “E” and “F”). The trash enclosure will have a rain-proof roof and wire mesh between the roof and walls to deter unauthorized entry. The design of the enclosure was coordinated with the Public Works Department to address truck access and capacity for trash, recycling, and organic waste. The Other improvements include new signs, new asphalt paving, accessible walkways between the building and the trash enclosure at the public right-of-way, and landscaping (see Exhibit “G”).

Dutch Bros proposes to operate from 5:00 a.m. to 11:00 p.m., daily. The application states the proposed facility may extend business hours of operation to 24 hours on a seasonal or permanent basis in the future. The proposed Resolution does not contain restrictions in the hours of operation. However, as delivery trucks will utilize the loading space at the north side of the site, the proposed Resolution does contain a condition that deliveries shall not occur between 10:01 p.m. and 6:00 a.m.

Code Requirements

Section 9401 of the LMC establishes the basis for approval or denial of a conditional use permit. LMC 9401.A sets forth the principles and standards that each case must comply with in order to be granted a CUP. A review of this application according to those standards reveals the following:

1. *The proposed conditional use shall not be in substantial conflict with the General Plan.*

The General Plan designates the site for Commercial uses. The subject use is commercial in nature and, therefore, would not be in conflict with the General Plan.

2. *The nature, condition, and development of adjacent uses, buildings, and structures shall be considered, and no Conditional Use Permit shall be permitted where such will adversely affect or be materially detrimental to said adjacent uses, buildings, or structures.*

The proposed project will not be materially detrimental to adjacent uses, buildings, or structures. Due to the site’s proximity to single-family homes (220 feet at the closest point), the proposed Resolution contains a condition all drive-thru menu boards and speakers to have adjustable volume controls so that sound generated by the drive-thru will not be audible at the closest property line. The proposed Resolution includes a condition requiring that all rooftop mechanical equipment be screened from view at ground level. The applicant shall submit a line of site analysis and revise the plans, if necessary, through means such as increasing the height of the parapet as necessary, to

comply with this condition.

3. *The site for a Conditional Use Permit shall be adequate in size and shape to accommodate the yards, walls, fences, parking and loading facilities, landscaping and other development features required by this chapter, or as required as a condition in order to integrate such uses with the uses in the neighborhood.*

LMC Section 9490.U.2 sets the parking requirement for fast-food restaurants. The parking required is one space per 100 square feet of gross floor area, or one space per three seats, whichever provides the greater number of spaces, provided that a minimum of five spaces are provided. The proposed building is 940 square feet, and the applicant has proposed eight (8) seats on the patio that will be protected by bollards. Therefore, the parking required for this use is 10 spaces and 35 spaces are proposed. There is sufficient parking to accommodate the proposed use.

The proposed landscaping plans will comply with zoning code requirements for minimum landscape area and minimum number of trees. Further analysis will be conducted during the building plan check process to ensure compliance with water conservation in landscaping regulations.

4. *The site for the conditional use is served by highways or streets adequate in width and improved as necessary to carry the kind and quantity of traffic such use would generate.*

The site is served by Lakewood Boulevard, South Street, and a public alley. The General Plan Circulation Element classifies both streets as primary arterial roadways. The Lakewood 2020 Traffic Census shows that the segment of Lakewood Boulevard between Ashworth Street and South Street had an average daily traffic (ADT) count of 32,585 vehicles, and the segment of South Street between Downey Avenue and Lakewood Boulevard had an average daily traffic (ADT) count of 21,375 vehicles. A traffic study was prepared for this project, and it found that a local operations analysis is not needed due to the low net trip generation and a detailed VMT assessment is not required for the project since it is a local-serving use.

A queuing study has been prepared for this project. The queue length for approximately 19 vehicles is expected to be adequate to accommodate the traffic generated by the proposed use. Staff and the Public Works Department reviewed the queuing study and made recommendations which have been included in the proposed plans. Those recommendations included improved access for the City's waste disposal contractor to access the trash enclosure and reducing the width of the southerly driveway at Lakewood Boulevard to create an exit-only driveway to prevent vehicle traffic from backing into the public right-of-way.

No significant change in traffic is anticipated as a result of this application. Lakewood Boulevard, South Street, and the public alley are adequate in width and improved as

necessary to accommodate the traffic that might be generated by the proposed conditional use.

5. *That all other conditions and limitations imposed by this Chapter in connection with the proposed application for a conditional use permit have been met.*

All other zoning standards for drive-thru restaurants are satisfied. The site provides the required 10' by 25' loading space adjacent to the trash enclosure.

Development Review Board

The Development Review Board (DRB) recommended approval of this project, Case No. 9251, to the Planning and Environment Commission, in a letter dated April 18, 2025. The DRB letter, including recommended conditions of approval, is included in this agenda packet. The proposed DRB conditions of approval have been incorporated by reference in the proposed Resolution.

CEQA

The project is Categorical Exemption (Categorically Exempt from the California Environmental Quality Act (CEQA) based on Sections 15301.(l).(3) and 15303.(c) of the CEQA Guidelines. Section 15301.(l).(3) exempts demolition of small commercial structures, and Section 15303.(c) exempts "A store, motel, office, restaurant or similar structure not involving the use of significant amounts of hazardous substances, and not exceeding 2,500 square feet in floor area. In urbanized areas, the exemption also applies to up to four such commercial buildings not exceeding 10,000 square feet in floor area on sites zoned for such use if not involving the use of significant amounts of hazardous substances where all necessary public services and facilities are available and the surrounding area is not environmentally sensitive."

Section 15303.(c) exempts "construction and location of...(c) A store, motel, office, restaurant or similar structure not involving the use of significant amounts of hazardous substances, and not exceeding 2500 square feet in floor area. In urbanized areas, the exemption also applies to up to four such commercial buildings not exceeding 10,000 square feet in floor area on sites zoned for such use if not involving the use of significant amounts of hazardous substances where all necessary public services and facilities are available and the surrounding area is not environmentally sensitive."

Public Notice

Notification of the public hearing for Conditional Use Permit No. 1026 was mailed to property owners within a 300-foot radius on March 20, 2025, was posted on the City's website on March 20, 2025, and was posted on March 20, 2025 pursuant to Section 9422, et seq., of the Lakewood Municipal Code and State Law. This public hearing is continued from the April 3, 2025 PEC meeting.

ATTACHMENTS

- 1) Traffic Study dated October 10, 2024
- 2) Queuing Technical Memorandum dated August 8, 2024

Conditional Use Permit No. 1026

May 1, 2025

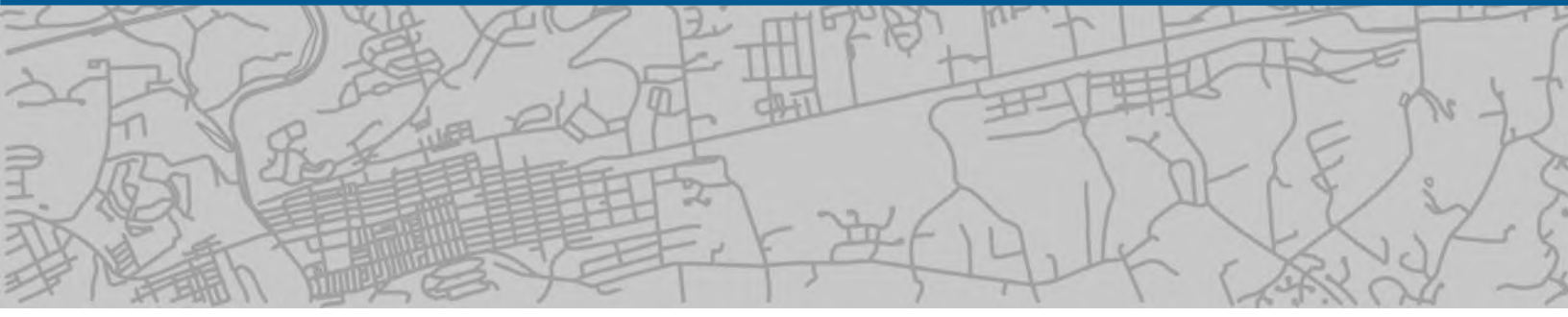
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- 3) Development Review Board letter dated April 18, 2025
- 4) Vicinity Map
- 5) Aerial View
- 6) Exhibit "A" – Site Plan
- 7) Exhibit "B" – Floor Plans
- 8) Exhibit "C" – Exterior Elevations
- 9) Exhibit "D" – Roof Plan
- 10) Exhibit "E" – Trash Enclosure Floor Plan
- 11) Exhibit "F" – Trash Enclosure Elevations
- 12) Exhibit "G" – Conceptual Landscape Plan
- 13) Proposed Resolution of Approval

DUTCH BROS TRAFFIC STUDY

Lakewood, CA

October 10, 2024



Dutch Bros Traffic Study Lakewood, CA

Prepared for:
Dutch Bros
110 SW 4th Street
Grants Pass, OR 97526

Prepared by:
Kittelson & Associates, Inc.
750 The City Drive, Suite 410
Orange, CA 92868
714.468.1997

Project Manager:
Samantha Liu
Engineering Associate

Project Principal:
Mychal Loomis
Principal Engineer

Project Number 30388.006

October 10, 2024





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INTRODUCTION

Dutch Bros is proposing to locate a Dutch Bros coffee shop at 5809 Lakewood Boulevard, Lakewood, California. The 950 square feet (sf) Dutch Bros with two drive-through lanes is proposed to replace a vacant restaurant building that shares its parking lot and site access with Walgreens.

Kittelson & Associates, Inc (Kittelson) prepared this traffic study for Dutch Bros to determine the expected transportation-related effects of the project. The scope of the study was developed in coordination with Dutch Bros and the City of Lakewood.

The traffic study documented in this report was performed to comply with CEQA transportation vehicle miles travelled (VMT) analysis and to assess transportation effects and consistency with City of Lakewood traffic standards and guidelines. The report covers the following traffic analyses:

- Project trip generation and trip distribution
- On-site queuing and circulation and safety assessment

PROJECT CONTEXT

The existing conditions of nearby roadways and intersections are evaluated to gain an understanding of the project site area. The existing conditions review involves mapping, reviewing, and documenting the existing land use, multi-modal transportation infrastructure, and roadway characteristics surrounding the project area. The findings from the existing conditions review are summarized below.

Project Location

The project shares a parking lot with Walgreens and is across the street from other small commercial plazas that include other local destinations such as Mobil Gas Station, Lakewood Car Wash, and Jack in the Box. Residential communities surround the project site in all directions. The project location can be visualized in Figure 1.

Figure 1: Site Location



SOURCE: KITTELSON & ASSOCIATES, INC (2024)

ROADWAY FACILITIES

The project site is west of the intersection of Lakewood Boulevard and South Street. Both roadways provide access to the parking lot which provides access to the project site. More information on current roadway configuration per the City of Lakewood's General Plan¹ Roadway Classification is as follows:

- **Lakewood Boulevard** is classified as a Major Highway and, within the site vicinity, has three through lanes in each direction with dedicated turn lanes at intersections. The roadway travels north-south through the city and connects drivers with land uses between State Route 91 and Long Beach Airport. The posted speed limit on this roadway is 40 miles per hour (mph). This roadway provides two access points to the parking lot in which the project is located.
- **South Street** is classified as a Major Highway and, within the site vicinity, has two through lanes in each direction with dedicated turn lanes at intersections. The roadway travels east-west through the city and connects drivers with land uses between Interstate 605 and Atlantic Avenue. The posted speed limit on the roadway is 40 mph. This roadway provides one access point to the parking lot in which the project is located.

PEDESTRIAN FACILITIES

The existing pedestrian network provides access between the project site and nearby land uses. Sidewalks are available on both sides of Lakewood Boulevard and South Street. Marked standard crosswalks are available on all legs of the intersection of Lakewood Boulevard and South Street.

BICYCLE FACILITIES

South Street has Class II bike facilities (bike lanes) on both sides of the roadway. No bike facilities are present on Lakewood Boulevard.

TRANSIT FACILITIES

Los Angeles Metro (LA Metro) and Long Beach Transit both operate within the project vicinity. LA Metro operates Route 266 along Lakewood Boulevard. Long Beach Transit operates Routes 111, 112, and 192 along Lakewood Boulevard and South Street.

- **Route 266**, operated by LA Metro, travels along Lakewood Boulevard connecting Sierra Madre Villa Station to the Lakewood Center Mall. The route includes stops in each direction at Lakewood Boulevard and South Street. Buses are scheduled to arrive in 20-minute intervals Monday through Friday, and 30-minute intervals on weekends and holidays.
- **Route 111**, operated by Long Beach Transit, is a loop route that travels between Long Beach Transit Gallery, Long Beach Airport, and Lakewood Center Mall. The route includes one stop along Lakewood Boulevard (heading southbound) and one stop along South Street (heading eastbound). Buses are scheduled to arrive at 60-minute intervals daily.
- **Route 112**, operated by Long Beach Transit, is a loop route that travels between Long Beach Transit Gallery, Long Beach Airport, and Lakewood Center Mall. The route is similar to Route 111 except from the Lakewood Center Mall it travels down to Highway 1 Along Clark Avenue and does not stop

¹ <https://www.lakewoodcity.org/files/assets/public/v/1/about/documents/lakewoodgenplan.pdf>

at the Long Beach Airport. The route includes one stop along Lakewood Boulevard (heading southbound) and one stop along South Street (heading eastbound). Buses are scheduled to arrive at 60-minute intervals daily.

- **Route 192**, operated by Long Beach Transit, travels along South Street connecting Loss Cerritos Center and Del Amo Station. The route includes stops in each direction at Lakewood Boulevard and South Street. Buses are scheduled to arrive in 30-minute intervals Monday through Friday, and 40-minute intervals on weekends and holidays.

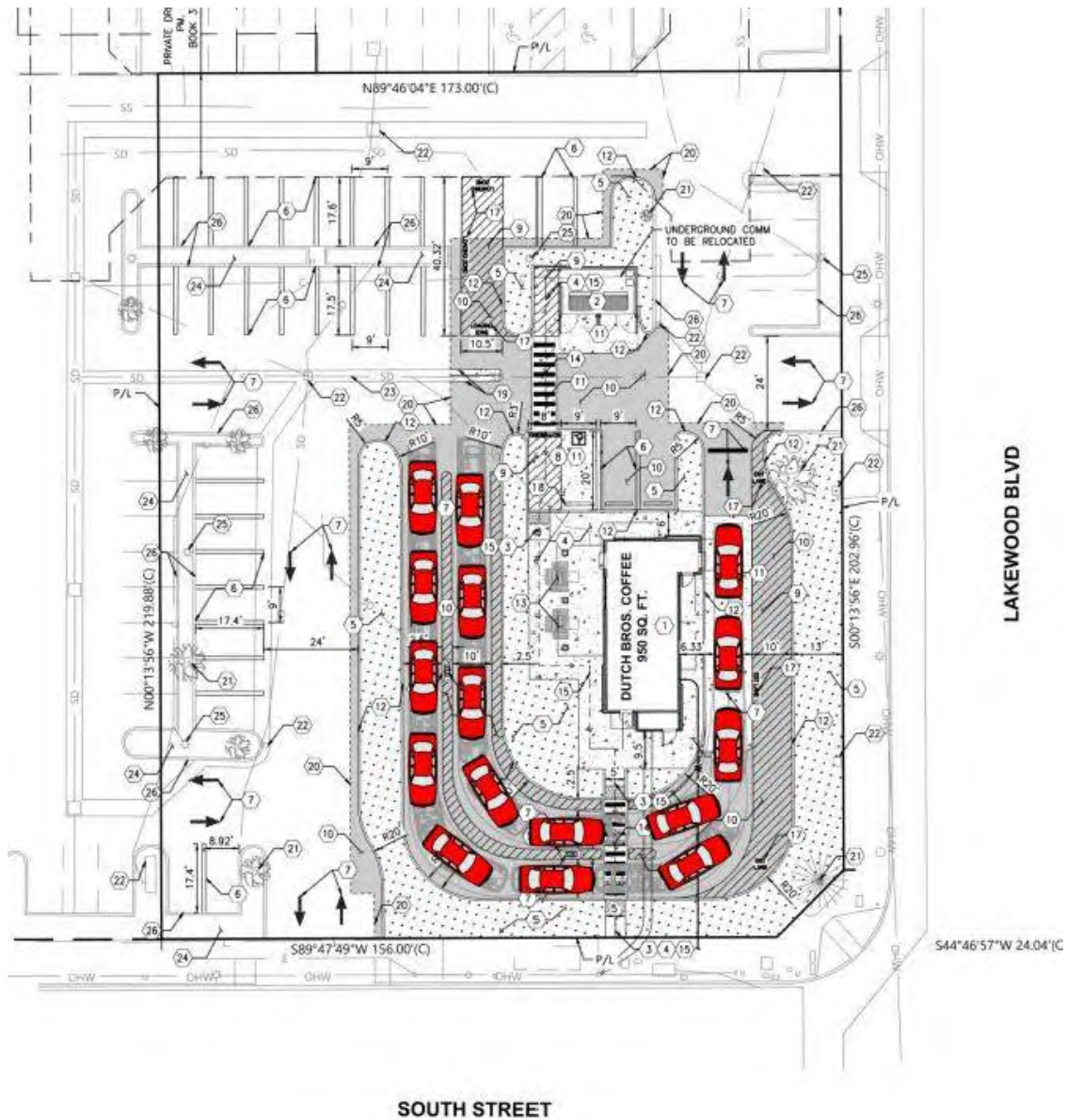
Proposed Project

The proposed project would replace a vacant restaurant building that shares its parking lot and site access with Walgreens. The current site has five driveways giving vehicular access – two from Lakewood Boulevard, one from South Street, and two along the alley north of Walgreens. The project site would consist of a 950-sf retail structure, three vehicle parking stalls including one ADA stall, and a vehicle drive-through served by a single service window. The drive-through has two lanes that converge at the service window and is designed to accommodate a queue of 16 vehicles at one time (see Figure 2). It is anticipated for the project to be operational by 2025.

A more detailed site plan is provided in Appendix A.

The project will have a single ingress driveway accessible from the existing parking lot. There is a total of five vehicle access points to the parking lot. Lakewood Boulevard provides two vehicle access points, South Street provides one vehicle access point, and the north alley by Walgreens provides two more vehicle access points.

Figure 2: Preliminary Site Plan



SOURCE: KITTELSON & ASSOCIATES, INC (2024); SITE PLAN DRAWN BY BARGHAUSEN CONSULTING ENGINEERS (2024)

PROJECT TRAFFIC GENERATION

Project-related trip estimates were calculated to assess the project's traffic impact on local roads. The information presented in this section was discussed and validated by City of Lakewood staff.

Submitted and approved traffic study scoping memorandum as of August 1, 2024 is provided in Appendix B.

Trip Generation

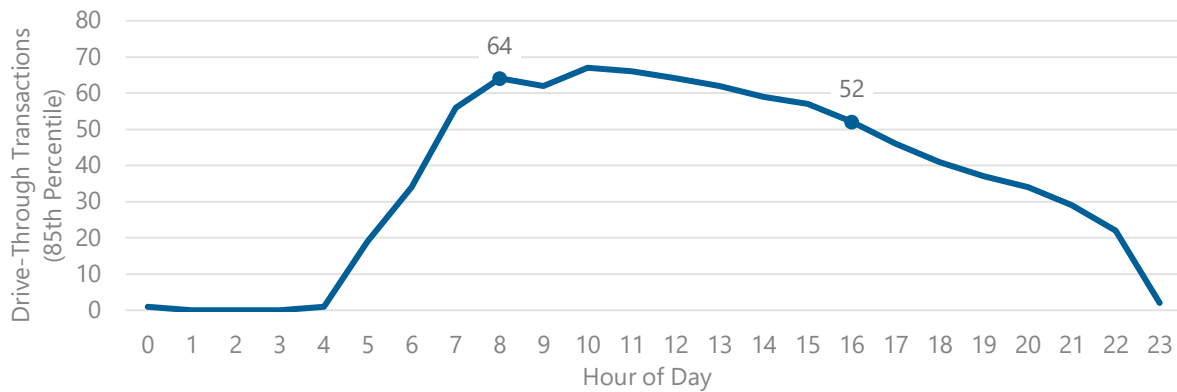
Trip generation for the Dutch Bros project uses a combination of the Institute of Transportation Engineers (ITE) *Trip Generation Manual* and historical drive-through transaction data of existing similar Dutch Bros. The similar Dutch Bros selected based on market service, layout, and traffic conditions are at the following locations:

- 81-776 Highway 111, Indio, CA 92201
- 44175 Jefferson St, La Quinta, CA 92253
- 32690 Yucaipa Blvd, Yucaipa, CA 92399
- 21690 Bear Valley Rd, Apple Valley, CA 92308
- 2560 E Palmdale Blvd, Palmdale, CA 93550
- 14788 Bear Valley Rd, Victorville, CA 92395
- 15211 Hook Blvd, Victorville, CA 92394

The ITE Trip Generation Manual was used to develop the proportional traffic inflow and outflow rates experienced during weekday morning (AM) and afternoon (PM) peak hours and pass-by rate assumptions. The 85th percentile of historical hourly Dutch Bros transaction data between April 1, 2023 and March 31, 2024 were used to approximate the total inbound and outbound trips throughout an average weekday and determine the weekday AM and PM peak hours.

The hourly historical transaction data for each location is provided in Appendix C.

Figure 3 visualizes the 85th percentile averaged across the similar Dutch Bros stores mentioned prior. As shown in the figure, the average number of transactions between the stores during the AM network peak period between 7:00 – 9:00 is 64, and the average during the PM network peak period between 4:00 – 6:00 is 52 transactions. It was assumed that each transaction represents one vehicle, although multiple transactions could occur within one vehicle.

Figure 3: Dutch Bros Daily Drive-Through Transactions Averaged Across All Locations

SOURCE: DUTCH BROS (2024)

Pass-by trips are a crucial aspect in trip generation for coffee shops since they are likely to receive similar or more pass-by trips than primary trips. A pass by trip reduction of 75% for the AM Peak Hour, PM Peak Hour, and daily traffic was applied, which is the average of the pass-by trip rates from ITE Trip Generation Manual for Land Use 934: Fast-Food Restaurant with Drive-Through Window and Land Use 938: Coffee/Donut Shop with Drive-Through Window and No Indoor Seating.

The site anticipates having seven to nine employees on site throughout the day. At peak times, four of the employees will be outside with one controlling traffic. These employees are on a shift schedule assumed to commute to the site outside of typical AM and PM peak hours so are not represented in the peak hour but are represented in the daily calculations.

Table 1 presents the inbound/outbound patterns used in the analysis and Table 2 presents the resulting trip generation estimates.

Table 1: Weekday Inbound and Outbound Rates for Trip Generation

Land Use	AM Peak Hour		PM Peak Hour		Daily	
	In	Out	In	Out	In	Out
Coffee/Donut Shop with Drive-Through Window and No Indoor Seating	50%	50%	50%	50%	50%	50%

SOURCE: INSTITUTE OF TRAFFIC ENGINEERS TRIP GENERATION MANUAL, 11TH EDITION**Table 2: Weekday Project Trip Generation Estimates**

Trip Type	AM Peak Hour			PM Peak Hour			Daily		
	In	Out	Total	In	Out	Total	In	Out	Total
Project Trip Generation ¹	64	64	128	52	52	104	892	892	1,784
Pass-By Trip Credits ²	(48)	(48)	(96)	(39)	(39)	(78)	(669)	(669)	(1,338)

Net Trip Generations	16	16	32	13	13	26	223	223	446
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Note:

¹ Total inbound/outbound trip generation estimates are the maximum number of historical transaction data within the hour across the similar Dutch Bros sites.

² A pass-by trip rate of 75% was used for AM Peak Hour, PM Peak Hour, and Daily Traffic.

SOURCE: DUTCH BROS (2024), COMPILED BY KITTELSON & ASSOCIATES, INC (2024)

Trip Distribution

The proposed project's trip distribution was developed based on a review of the adjacent roadway network and surrounding land uses to determine anticipated origins and paths of travel. Vehicle trips are separated between primary and pass-by trips.

Detailed trip distributions can be visualized in Appendix D.

PRIMARY TRIPS

Primary trips to and from the site were distributed to account for attracting local community trips. Inbound and outbound trip patterns generally consist of the following distribution:

- 25% of trips to and from local roads west of the project site
- 15% of trips to and from local roads north of the project site
- 35% of trips to and from local roads south of the project site
- 25% of trips to and from local roads east of the project site

PASS-BY TRIPS

Pass-by trips to and from the site account for attracting trips already occurring along South Street and Lakewood Boulevard that now would divert their original path to stop at the proposed project site. It is assumed 15% and 35% of pass-by trips would be generated by southbound and northbound traffic, respectively, along Lakewood Boulevard. Along South Street, it is assumed 25% of pass-by trips would be generated by both eastbound and westbound traffic.

Trip Assignment

The trip generation volumes were applied to the trip distribution to calculate the number of vehicle-trips the project would add to the surrounding roadway network. The following study intersections, also visualized in Figure 4 were selected due to their proximity and likely direct impact to the site:

1. Lakewood Boulevard and South Street
2. South Street and Project Driveway
3. Walgreens Alley and Walgreens Left Driveway
4. Walgreens Alley and Walgreens Right Driveway
5. Lakewood Boulevard and North Driveway
6. Lakewood Boulevard and South Driveway

The total net new project trip assignment for the study area intersections during the weekday AM and PM peak hours are shown in Figure 5 and Figure 6, respectively. Provided that the net trip assignments at each intersection were relatively low, a local operations assessment was determined to not be needed since the additional traffic will have a nominal effect.

Figure 4: Study Intersections



SOURCE: KITTELSON & ASSOCIATES, INC (2024)

Figure 5: Net New Project Trip Assignment – AM Peak Hour

SOURCE: KITTELSON & ASSOCIATES, INC (2024)

Figure 6: Net New Project Trip Assignment – PM Peak Hour

SOURCE: KITTELSON & ASSOCIATES, INC (2024)

SITE PERFORMANCE ANALYSES

The existing roadway conditions and proposed site plan were assessed to determine if on-site safety or operational improvements were necessary due to an increase in traffic from the project.

Site Access and On-Site Circulation

When reviewing the proposed site access and on-site circulation, the following details were noted:

- **Parking.** The project would reconfigure the parking lot within its parcel and provide a total of 36 parking spaces. Parking would not be regularly needed by customers since it is primarily a drive-through operation. The parking spaces would be available as part of a shared lot for users of the shopping center where the project is to be located, and collectively is assumed to suffice to meet the needs of the project for employees and any walk-up customers.
- **Driveway Impacts.** Exiting queues at the south driveway along Lakewood Boulevard may disrupt on-site circulation, particularly by blocking vehicles from exiting the drive through. More than one queued vehicle exiting the shopping center at this driveway can impede project drive through exiting vehicles, while queues over four vehicles would also interfere with drive-through entry and access. The south driveway to Lakewood Boulevard is right-in, right-out access so vehicles exiting would only need to find a gap in southbound traffic on Lakewood Boulevard. Dutch Bros should monitor – especially during peak operation periods – this driveway adjacent to their drive through exit and reroute vehicles to the north driveway on Lakewood Boulevard that also provides right-out only access to Lakewood Boulevard. A more permanent reroute of vehicles exiting the drive through could be installed if this becomes an ongoing issue.
- **Sight Distance.** Sight distance from the drive-through entrance and exit were assessed, and no anticipated issues were found. The site plan confirms that there would not be any landscaping or other installations obstructing sightlines. There is an existing tree on the east side of the drive through lane exit but it should not prohibit visibility for vehicles exiting as there is adequate throat in the exit lane that positions vehicles to have adequate monitoring of approaching cyclists, pedestrians, or vehicles in the parking aisle.
- **Drive Through Vehicle Access.** The site plan provides visibility for vehicles approaching the entrance from any direction to see other oncoming vehicles and to see the existing queue to determine the appropriate drive through lane to enter. Vehicles approaching the drive through lane from the South Street driveway would be required to make a U-turn into the drive through lane but is not anticipated to be an issue. The drive-through entrance is surrounded by parking and would not conflict with other on-site operations.
- **Adequacy of Pedestrian Facilities.** Pedestrian access to the site would include existing sidewalks, crosswalks and walkways along the project frontage that would facilitate pedestrian access. There are standard crosswalks across all legs at the Lakewood Boulevard / South Street intersection.
- **Bicycle Accessibility.** South Street has Class II bike facilities on both sides of the roadway. Bicyclists would use the drive aisles to access the project site.
- **Accessibility from Adjacent Transit Stops.** Four bus routes have stops within 500 feet of the project site. One is operated by LA Metro (Route 266) and the other three (Routes 192, 111, 112) are operated by Long Beach Transit. Riders alighting Route 266 and 192, operated by LA Metro and Long

Beach Transit, respectively, have direct access to the project site. Riders alighting Routes 111 and 112 need to cross at Lakewood Boulevard / South Street to access the project site. Sidewalks are provided from the bus stops to the project site.

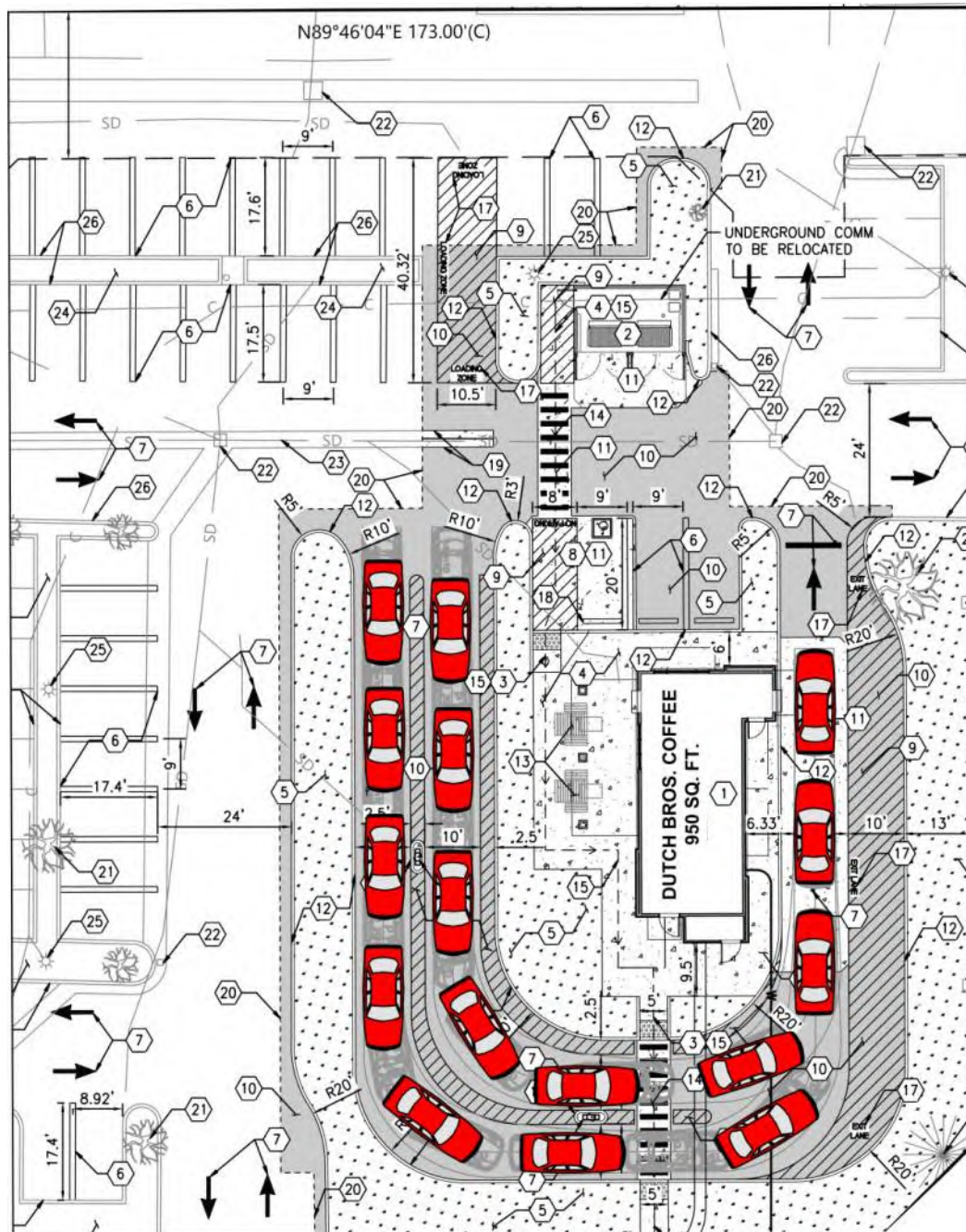
Drive-Through Queuing

Queuing at coffee shop drive-throughs often are where potential impacts to the roadway network occur. To estimate the potential queueing conditions and how often it may affect public right-of-way, an M/M/1 queueing model was used. This model assumes that customer arrivals and service times are random and requires the average arrival and service rates as inputs. Using historical transaction data from similar Dutch Bros stores, 67 and 80 vehicles per hour were used as conservative arrival and service rates, respectively, for the proposed store.

From the model, it was determined that there is approximately a 5% probability that the number of vehicles waiting to be served will be longer than 16 vehicles, as shown in Figure 7. Since the drive-through can accommodate up to 16 vehicles, it is expected that queues would not affect the public right of way during the highest 5% of demand times. However, it is still recommended for Dutch Bros to implement the following strategies upon reaching 75% of drive-through capacity (approximately 12 vehicles) to manage queues as efficiently and effectively as possible:

- Dutch Bros staff takes orders and payments from and makes deliveries to the queued drive-through lanes to minimize the wait time at the service window. The site plan includes a queue exit lane adjacent to the pick-up window. This is an important queue length management feature because it allows vehicles that receive their fulfilled orders prior to reaching the pick-up window to exit out of the queue early, thereby reducing the overall length of the queue.
- Dutch Bros staff can place signage to guide non-Dutch Bros customer vehicles away from the drive-through entrance and act as temporary traffic control personnel who can effectively manage queues and prevent blockage situations.
- Parking on site can be used as waiting areas for vehicles that make large orders. This is an important operations factor that provides the ability to remove vehicles with long order fulfillment times from the queue and provide a significant positive effect on the overall queue length.

It should be noted that accurately estimating total latent demand is difficult due to various factors. These factors include the location, type, convenience, and pricing of competing opportunities in the area, as well as the traffic volume on adjacent streets and the socioeconomic characteristics of the nearby population and employment areas.

Figure 7: 95th Percentile Queuing Scenario

SOURCE: KITTELSON AND ASSOCIATES (2024); SITE PLAN IS DRAWN BY BARGHAUSEN CONSULTING ENGINEERS (2024)

APPENDIX A

Site Plan

"The name DUTCH BROS., and all associated logos, distinctive designs, content, information, and other materials featured, displayed, contained herein, and made available by Dutch Bros., including but not limited to, the "look and feel" of the establishments and products, all text, images, colors, configurations, graphics, designs, illustrations, photographs, and pictures (collectively, the "Materials") are owned by and/or licensed by DB Franchising USA, LLC and are protected by copyright, trademark, trade dress, patent, and/or other intellectual property rights and unfair competition laws under the United States and foreign laws."

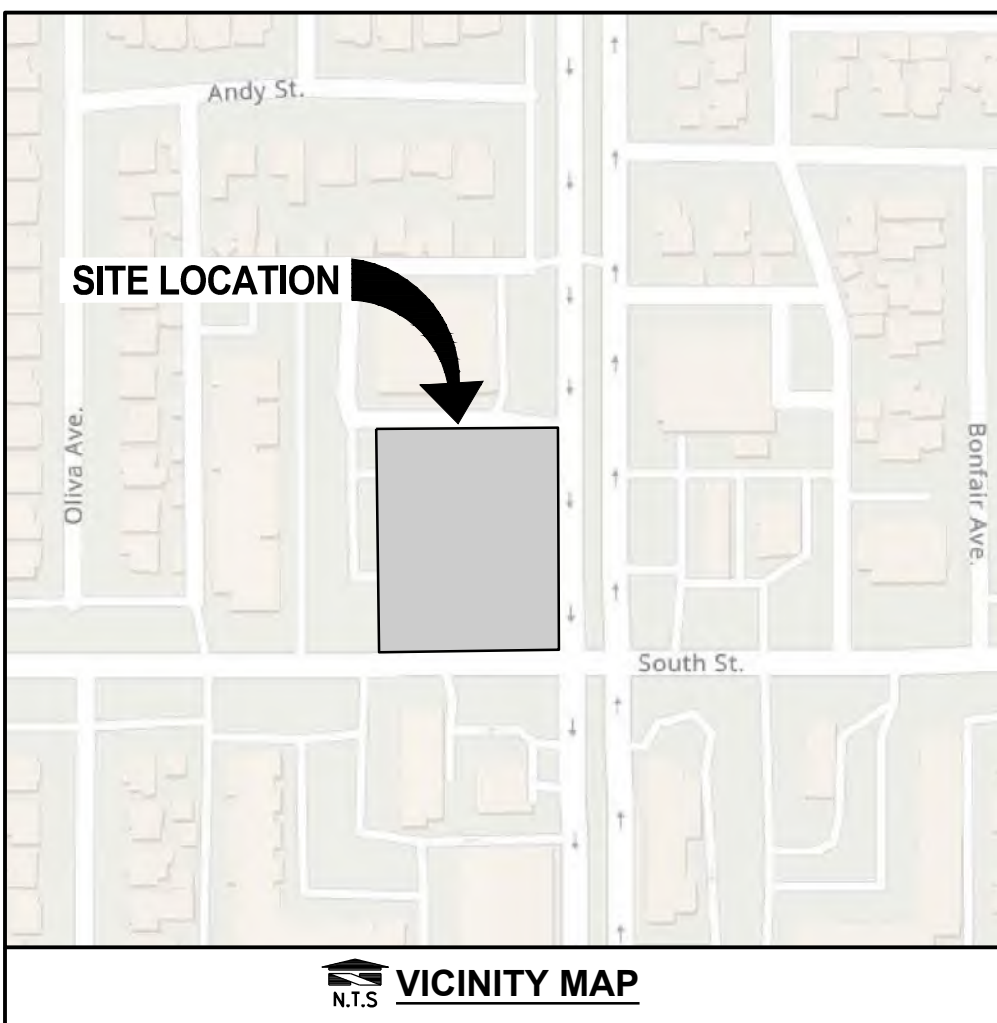
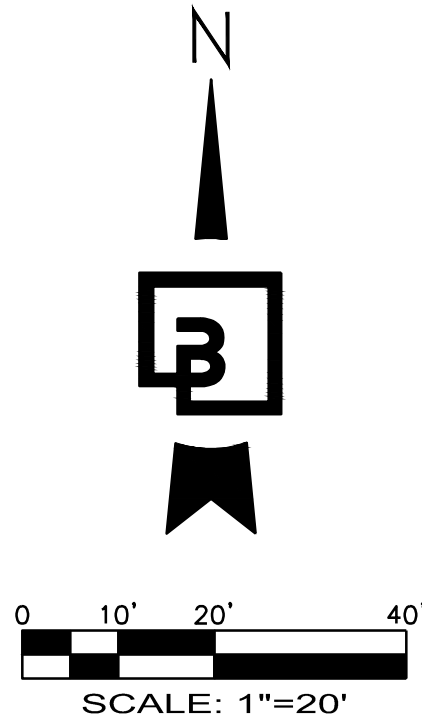


Know what's below.
Call before you dig.
Dial 811

DUTCH BROS COFFEE CA6405

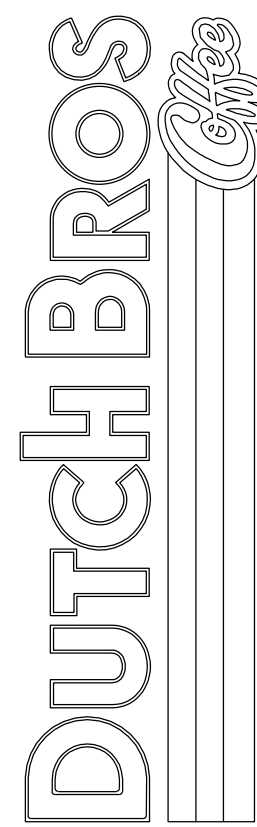
5809 LAKEWOOD BLVD., LAKEWOOD, CA

PRELIMINARY SITE PLAN

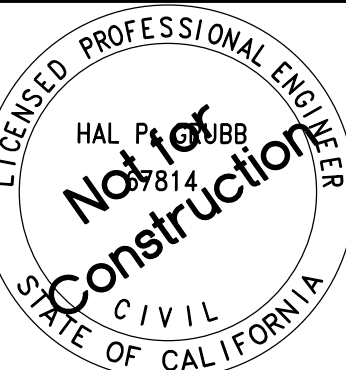


PRELIMINARY SITE PLAN
5809 LAKEWOOD BLVD.,
LAKEWOOD, CA

Title:

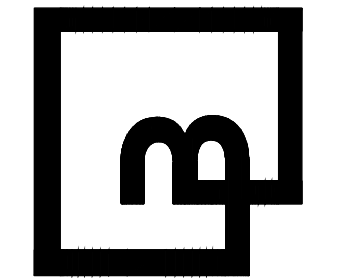


For:



Scale:	
Horizontal	1" = 20'
Vertical	N/A
Designed RAG	
Drawn	RAG
Checked	MTL
Approved	JAH
Date	7/15/24

Barghausen
Consulting Engineers, Inc.
18215 72nd Avenue South
Kent, WA 98032
425.251.6222
barghausen.com

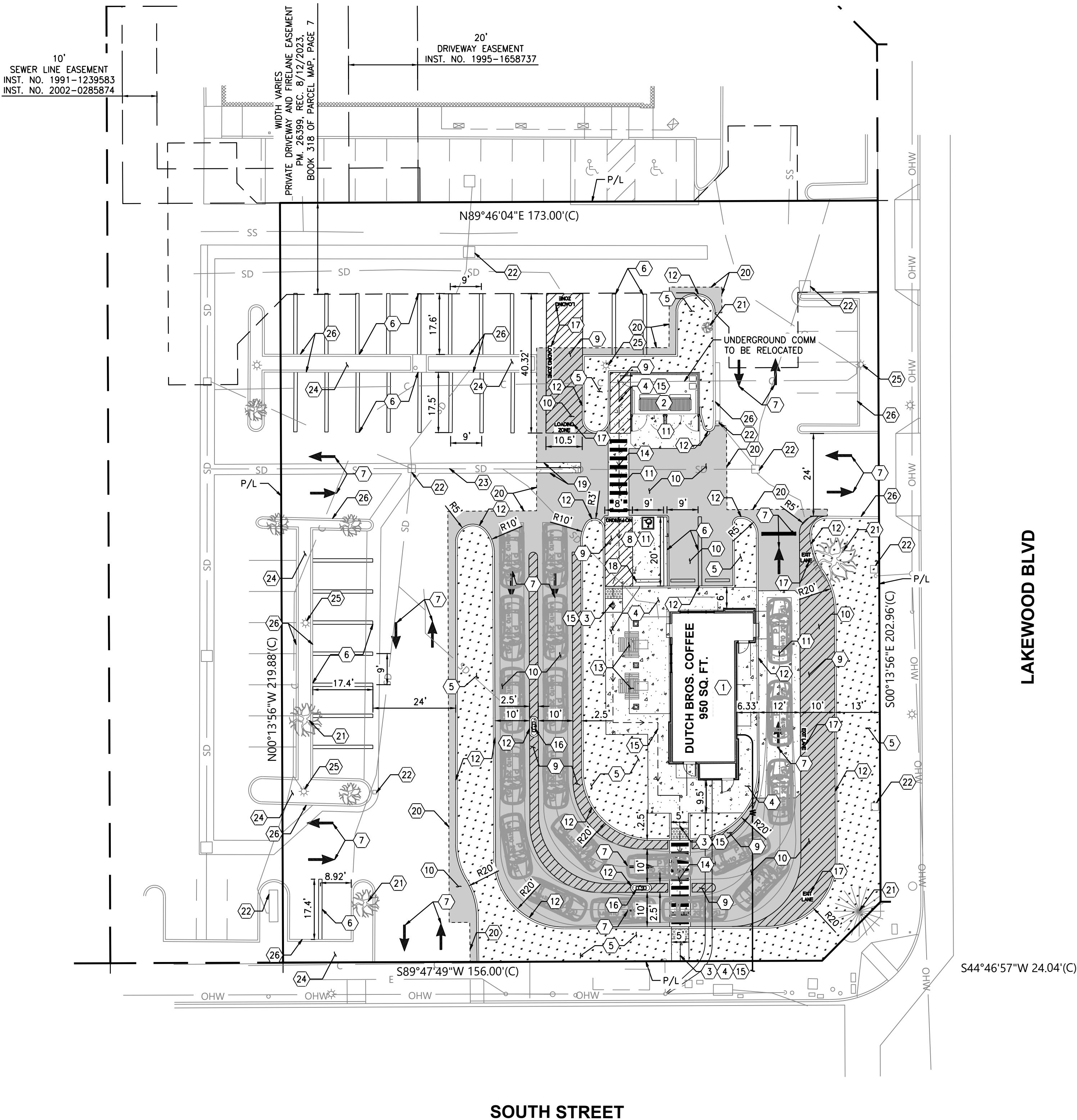


Job Number
23477

Sheet
1 OF 1

2019 DB
Franchising USA, LLC

P:\23000s\23477\preliminary\23477-PS.dwg 7/15/2024 12:41 PM JBARRENTOS



LAKEWOOD BLVD

S44°46'57"W 24.04'(C)

SOUTH STREET

CONSTRUCTION NOTES:

1. PROPOSED DUTCH BROS COFFEE STAND; REFER TO ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.
2. PROPOSED 24'X12' TRASH ENCLOSURE; REFER TO ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.
3. PROPOSED ADA RAMP.
4. PROPOSED PRIVATE CONCRETE SIDEWALK (WIDTH PER PLAN).
5. PROPOSED LANDSCAPING AREA; REFER TO LANDSCAPING PLANS FOR ADDITIONAL INFORMATION.
6. PROPOSED 4" WIDE WHITE REFLECTIVE PAINT PARKING STALL STRIPES. TYPICAL.
7. PROPOSED PAVEMENT TRAFFIC MARKINGS, TYPICAL.
8. PROPOSED ACCESSIBLE PARKING SPACE AND AISLE WITH ALL REQUIRED SIGNAGE.
9. PROPOSED PAVEMENT STRIPING: 4" WIDE, 24" O.C., 45 DEGREE ANGLE, SAFETY WHITE.
10. PROPOSED ASPHALT PAVING.
11. PROPOSED CONCRETE PAVING.
12. PROPOSED BARRIER CURB.
13. PROPOSED LOCATION OF FIXED 4 SEAT PICNIC TABLE.
14. PROPOSED CROSSWALK PAVEMENT STRIPING.
15. PROPOSED ACCESSIBLE PATH.
16. PROPOSED LOCATION SIGN.
17. PROPOSED 12" PAVEMENT LETTERING.
18. PROPOSED PRECAST WHEEL STOP, TYPICAL.
19. PROPOSED VALLEY GUTTER.
20. PROPOSED SAWCUT LINE.
21. EXISTING TREE, TYPICAL.
22. EXISTING UTILITY STRUCTURE AND ASSOCIATED UTILITIES SHOWN TO REMAIN.
23. EXISTING VALLEY GUTTER TO REMAIN.
24. EXISTING LANDSCAPING TO REMAIN.
25. EXISTING SITE LIGHT.
26. EXISTING CURB.

PROJECT DATA

DB2550-A1		
NAME: DUTCH BROS COFFEE - LAKEWOOD, CA		
ADDRESS: 5809 LAKEWOOD BLVD., LAKEWOOD, CA 90712		
PROPOSED PARCEL AREA = 37,902 SQ. FT. (0.87 ACRES)		
PARKING SPACES:		
- PROPOSED REGULAR PARKING	=	2
- PROPOSED ADA PARKING	=	1
- EXISTING TO REMAIN	=	33
- TOTAL PARKING	=	36
QUEUING = 19		
TRASH ENCLOSURE = 20' X 12'		
PROJECT AREA COVERAGE:		
BUILDING/CANOPY, TRASH ENCLOSURE	±1,190 SF	(6.27%)
PARKING AND MANEUVERING:	±9,561 SF	(50.35%)
WALKWAYS:	±2,818 SF	(14.84%)
LANDSCAPE:	±5,419 SF	(28.54%)
	18,988 SF	(100.0%)

LEGEND

BUILDING LINE	////
EXISTING CURB TO REMAIN	=====
PROPOSED CURB	=====
PROPOSED LANDSCAPING
PROPOSED ASPHALT	=====
PROPOSED CONCRETE

PRELIMINARY NOT FOR CONSTRUCTION

APPENDIX B
Scoping Memorandum
Approved on August 1, 2024

TECHNICAL MEMORANDUM

July 30, 2024

Project# 30388.006

To: Paul Kuykendall, AICP

City of Lakewood

5050 Clark Avenue

Lakewood, CA 90712

From: Kittelson & Associates, Inc

RE: Dutch Bros at 5809 Lakewood Boulevard – Scoping Memorandum

A 950 square feet Dutch Bros with a drive-through is proposed at 5809 Lakewood Boulevard, Lakewood, California. The Dutch Bros will replace a vacant restaurant building that shares its parking lot and site accesses with Walgreens. This technical memorandum outlines our proposed scope of work for the traffic study report to be submitted to the City of Lakewood. The findings from this memorandum will inform the next steps for the traffic study analysis.

This memorandum covers the following topics:

- Trip Generation Methods and Estimates
- Trip Distribution and Assignment
- VMT Screening and Assessment
- Next Steps in Analysis

PROJECT TRIP GENERATION

Trip generation for the Dutch Bros project uses a combination of the Institute of Transportation Engineers (ITE) *Trip Generation Manual* and historical drive-through transaction data of existing similar Dutch Bros. The similar Dutch Bros selected based on market service, layout, and traffic conditions are at the following locations:

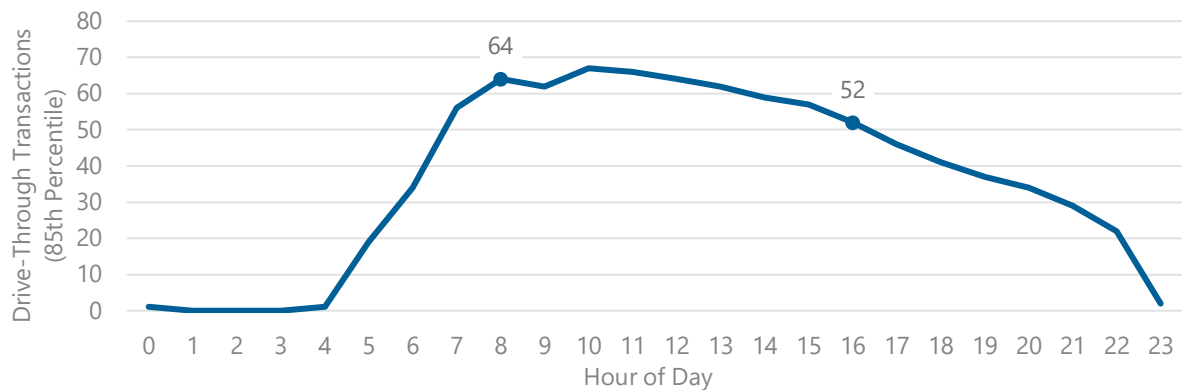
- 81-776 Highway 111, Indio, CA 92201
- 44175 Jefferson St, La Quinta, CA 92253
- 32690 Yucaipa Blvd, Yucaipa, CA 92399
- 21690 Bear Valley Rd, Apple Valley, CA 92308
- 2560 E Palmdale Blvd, Palmdale, CA 93550
- 14788 Bear Valley Rd, Victorville, CA 92395
- 15211 Hook Blvd, Victorville, CA 92394

The ITE Trip Generation Manual was used to develop the proportional traffic inflow and outflow rates experienced during weekday morning (AM) and afternoon (PM) peak hours and pass-by rate assumptions. The 85th percentile of historical hourly Dutch Bros transaction data between April 1, 2023 and March 31, 2024 were used to approximate the total inbound and outbound trips throughout an average weekday and determine the weekday AM and PM peak hours.

The hourly historical transaction data for each location is provided in Appendix A.

Figure 1 visualizes the 85th percentile averaged across the similar Dutch Bros stores mentioned prior. As shown in the figure, the average number of transactions between the stores during the AM network peak period between 7:00 – 9:00 is 64, and the average during the PM network peak period between 4:00 – 6:00 is 52 transactions. It was assumed that each transaction represents one vehicle, although multiple transactions could occur within one vehicle.

Figure 1: Dutch Bros Daily Drive-Through Transactions Averaged Across All Locations



SOURCE: DUTCH BROS (2024)

Pass-by trips are a crucial aspect in trip generation for coffee shops since they are likely to receive similar or more pass-by trips than primary trips. A pass by trip reduction of 75% for the AM Peak Hour, PM Peak Hour, and daily traffic was applied, which is the average of the pass-by trip rates from ITE Trip Generation Manual for Land Use 934: Fast-Food Restaurant with Drive-Through Window and Land Use 938: Coffee/Donut Shop with Drive-Through Window and No Indoor Seating.

The site anticipates having seven to nine employees on site throughout the day. At peak times, four of the employees will be outside with one controlling traffic. These employees are on a shift schedule assumed to commute to the site outside of typical AM and PM peak hours so are not represented in the peak hour but are represented in the daily calculations.

Table 1 presents the inbound/outbound patterns used in the analysis and Table 2 presents the resulting trip generation estimates.

Table 1: Weekday Inbound and Outbound Rates for Trip Generation

Land Use	AM Peak Hour		PM Peak Hour		Daily	
	In	Out	In	Out	In	Out
Coffee/Donut Shop with Drive-Through Window and No Indoor Seating	50%	50%	50%	50%	50%	50%

SOURCE: INSTITUTE OF TRAFFIC ENGINEERS TRIP GENERATION MANUAL, 11TH EDITION

Table 2: Weekday Project Trip Generation Estimates

Trip Type	AM Peak Hour			PM Peak Hour			Daily		
	In	Out	Total	In	Out	Total	In	Out	Total
Project Trip Generation ¹	64	64	128	52	52	104	892	892	1,784
Pass-By Trip Credits ²	(48)	(48)	(96)	(39)	(39)	(78)	(669)	(669)	(1,338)
Net Trip Generations	16	16	32	13	13	26	223	223	446

Note:

¹ Total inbound/outbound trip generation estimates are the maximum number of historical transaction data within the hour across the similar Dutch Bros sites.

² A pass-by trip rate of 75% was used for AM Peak Hour, PM Peak Hour, and Daily Traffic.

SOURCE: DUTCH BROS (2024), COMPILED BY KITTELSON & ASSOCIATES, INC (2024)

TRIP DISTRIBUTION

The proposed project's trip distribution was developed based on a review of the adjacent roadway network and surrounding land uses to determine anticipated origins and paths of travel. Vehicle trips are separated between primary and pass-by trips.

Detailed trip distributions can be visualized in Appendix B.

Primary Trips

Primary trips to and from the site were distributed to account for attracting local community trips. Inbound and outbound trip patterns generally consist of the following distribution:

- 25% of trips to and from local roads west of the project site
- 15% of trips to and from local roads north of the project site
- 35% of trips to and from local roads south of the project site
- 25% of trips to and from local roads east of the project site

Pass-by Trips

Pass-by trips to and from the site account for attracting trips already occurring along South Street and Lakewood Boulevard that now would divert their original path to stop at the proposed project site. It is assumed 15% and 35% of pass-by trips would be generated by southbound and northbound traffic, respectively, along Lakewood Boulevard. Along South Street, it is assumed 25% of pass-by trips would be generated by both eastbound and westbound traffic.

TRIP ASSIGNMENT

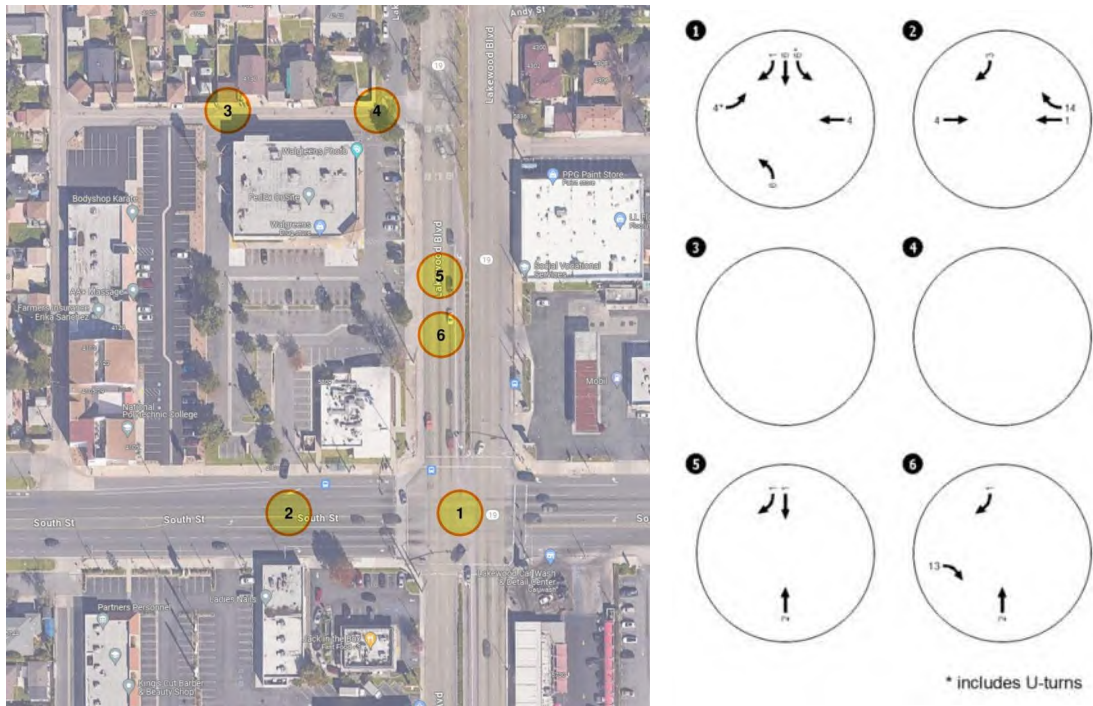
The trip generation volumes were applied to the trip distribution to calculate the number of vehicle-trips the project would add to the surrounding roadway network. The following study intersections, also visualized in Figure 2 were selected due to their proximity and likely direct impact to the site:

1. Lakewood Boulevard and South Street
2. South Street and Project Driveway
3. Walgreens Alley and Walgreens Left Driveway
4. Walgreens Alley and Walgreens Right Driveway
5. Lakewood Boulevard and North Driveway
6. Lakewood Boulevard and South Driveway

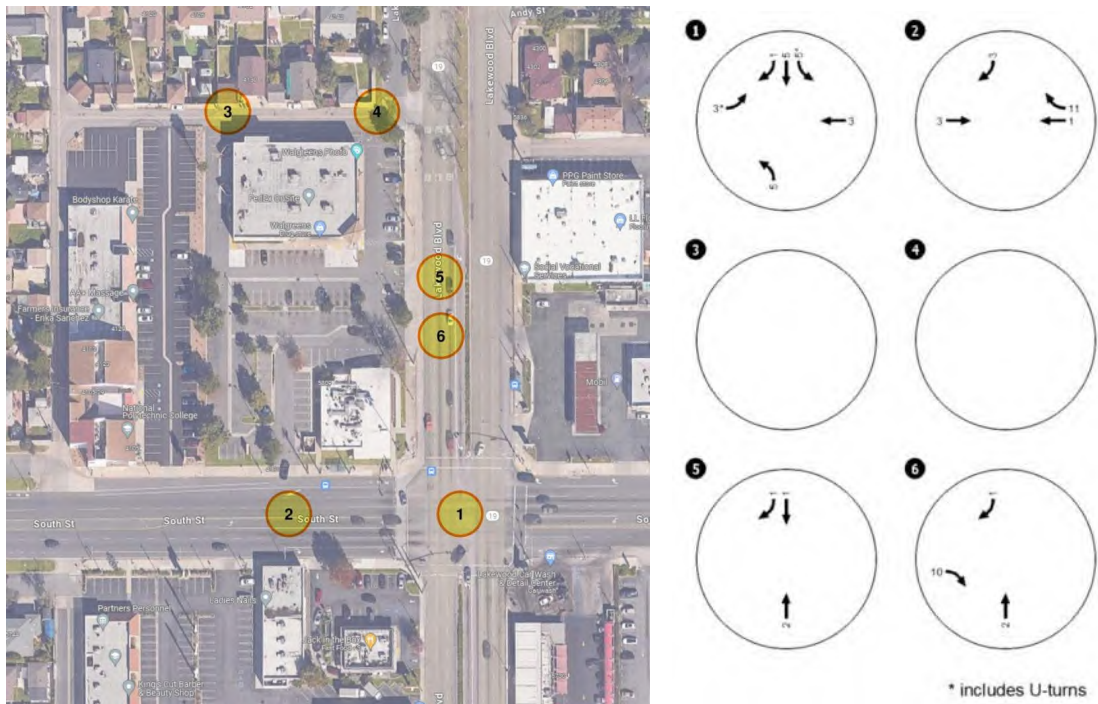
Figure 2: Study Intersections

SOURCE: KITTELSON & ASSOCIATES, INC (2024)

The total net new project trip assignment for the study area intersections during the weekday AM and PM peak hours are shown in Figure 3 and Figure 4, respectively. Provided that the net trip assignments at each intersection were relatively low, a local operations assessment is anticipated to not be needed since the additional traffic will have a nominal effect.

Figure 3: Net New Project Trip Assignment – AM Peak Hour

SOURCE: KITTELSON & ASSOCIATES, INC (2024)

Figure 4: Net New Project Trip Assignment – PM Peak Hour

SOURCE: KITTELSON & ASSOCIATES, INC (2024)

VEHICLE-MILES TRAVELLED (VMT) ASSESSMENT

The transportation analysis documented in this report was performed to comply with CEQA transportation VMT analysis. CEQA analysis requires an evaluation of project impacts related to VMT. However, a detailed CEQA assessment is not required for land use elements of a project that meet certain screening criteria, as described below.

At this time, the City of Lakewood has not adopted its own thresholds for VMT. Therefore, VMT guidelines from the Governor's Office of Planning and Research (OPR) Technical Advisory on Evaluating Transportation Impacts in CEQA (December 2018) were referenced and utilized as guidance.

Screening Criteria

CEQA analysis requires an evaluation of project impacts related to VMT. However, a detailed CEQA assessment is not required for land use elements of a project that meet certain screening criteria. To be screened out of a detailed VMT analysis, a project or project component would need to satisfy at least one of the VMT screening criteria. A summary of OPR's screening criteria and determinations are listed below:

- **Small Project Size:** Projects generating less than 110 trips per day may be considered to have an insignificant impact on VMT. This threshold is not VMT-based but relates to the CEQA categorical exemption for existing facilities and additions to existing structures up to 10,000 square feet.
- **Projects Within Transit Priority Areas:** Projects, including residential, retail, and office projects, as well as mixed-use projects within a ½ mile of an existing major transit stop or along a high-quality transit corridor, are generally presumed to have a minor impact on VMT. This presumption is not valid if project-specific or location-specific information indicates significant VMT levels. An existing major transit stop is defined as a site with a rail transit station, a ferry terminal served by bus or rail transit, or the intersection of multiple major bus routes with frequent service during peak commute periods.
- **Local-Serving Retail:** Projects categorized as local-serving retail are presumed to have an insignificant impact on VMT.
- **Redevelopment Projects Resulting in Net VMT Reduction:** Redevelopment projects that would decrease VMT, meaning the proposed land use generates less VMT than the existing use, may be considered to have an insignificant impact on VMT.
- **Affordable Housing:** The OPR's technical advisory provides special considerations for affordable housing. Projects that consist of 100% affordable housing in infill locations are presumed to have a minor impact on VMT. Infill locations generally offer better access to transit and more opportunities for walking and cycling. The definition of infill locations is determined based on local conditions.

Screening Determination

The project would be considered a local-serving retail, and thus is not anticipated to result in a significant VMT impact due to low long-term operational traffic.

Table 3: VMT Screening Summary

VMT Screening Criteria	Criterion Met?	Reasoning
Small Projects	No	The proposed project would generate up to 446 net new daily primary vehicle trips and therefore would not screen out of a detailed VMT analysis.
Projects Near High Quality Transit	No	The proposed project is not located in a near high-quality transit and is not screened out under this criterion.
Local-Serving Retail	Yes	The proposed project is considered local-serving retail and therefore would screen out of a detailed VMT analysis.
Affordable Housing	No	The project is not part of a residential project and is not screened out under this criterion.
Redevelopment Projects	No	The project would likely generate more daily total VMT since the land is currently open area and is not screened out under this criterion.

SOURCE: KITTELSON & ASSOCIATES, INC (2024)

NEXT STEPS

This memorandum was prepared to gain feedback from the City on the proposed methodology for the transportation study of the Dutch Bros. After confirming this proposed approach with City staff, Kittelson will proceed with an analysis of project drive-through queues and a qualitative assessment of site access and circulation. With the information provided in this scoping memorandum, it was concluded that a local operations analysis is not needed due to the low net trip generation and a detailed VMT assessment is not needed for the project since it is considered a local-serving retail. The findings of the queuing and on-site qualitative analyses will be documented in separate technical memorandum for City review.

APPENDIX C

Hourly Historical Transaction Data

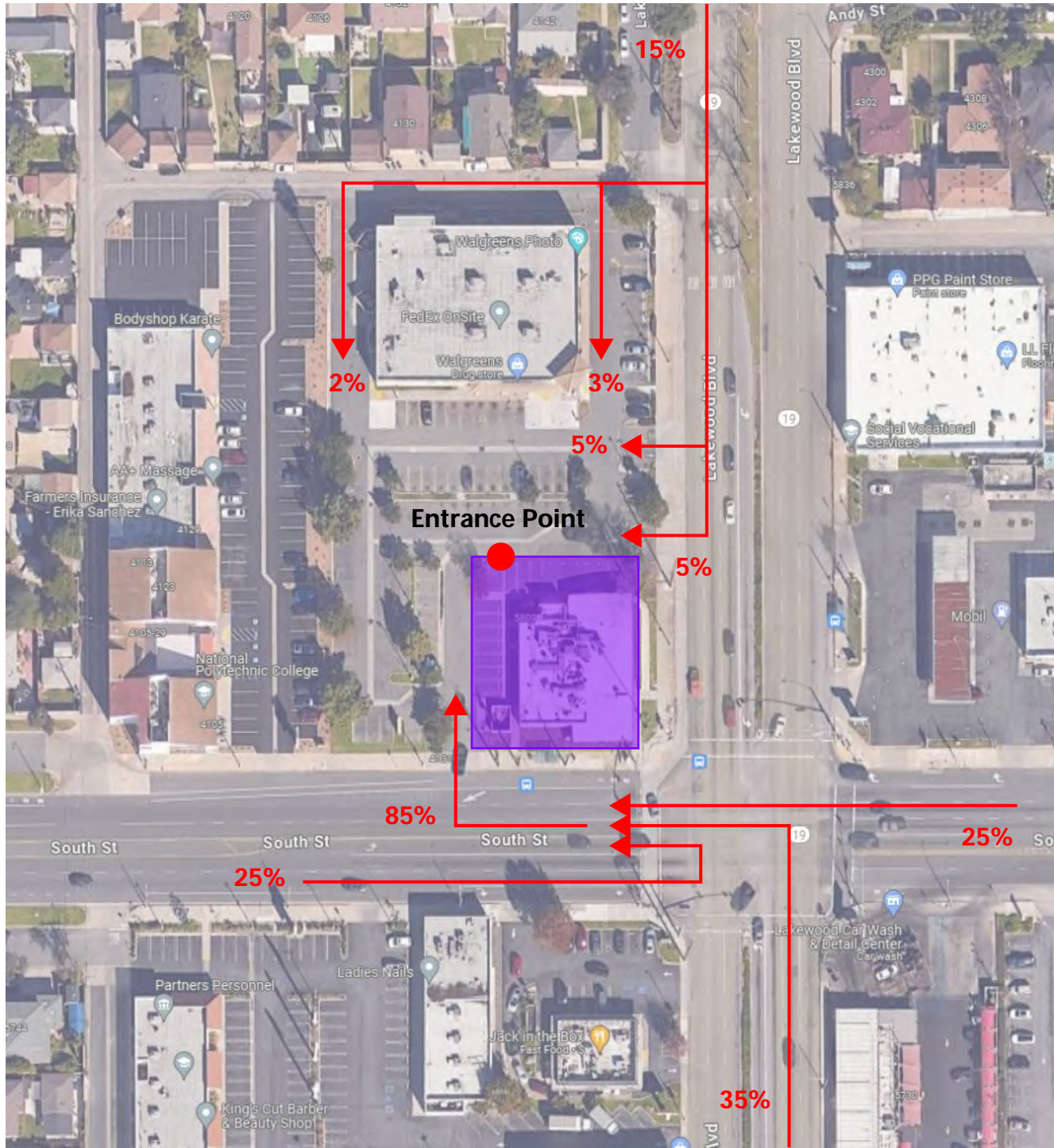
DUTCH BROS TRANSACTION DATA									
Average of Guests (Order)		85TH PERCENTILE							AVG OF LOCATIONS
Row Labels		Apple Valley CA4101	Indio CA3702	La Quinta CA3703	Palmdale CA3903	Victorville CA4102	0 CA4103	Yucaipa CA4501	
0		0.0	1.0	0.0	0.0	0.0	2.0	1.0	1.0
1		0.0	0.0	1.0	0.0	0.0	0.0	0.0	0.0
2		0.0	0.0	1.0	0.0	0.0	0.0	0.0	0.0
3		0.0	1.0	0.0	0.0	0.0	0.0	0.0	0.0
4		1.1	1.0	2.0	1.0	2.0	2.0	1.0	1.0
5		15.0	18.0	20.0	18.0	21.0	22.0	19.0	19.0
6		35.0	34.0	39.0	27.0	34.0	34.0	34.0	34.0
7		50.0	55.0	73.0	52.0	47.0	58.0	57.0	56.0
8		56.0	66.0	86.0	57.0	57.3	64.0	60.0	64.0
9		59.0	59.0	78.0	60.0	61.0	60.0	60.0	62.0
10		62.0	61.3	85.0	65.0	66.0	65.0	64.0	67.0
11		61.0	57.0	80.0	67.0	66.0	65.0	64.0	66.0
12		59.0	55.0	73.0	70.0	63.0	64.0	63.4	64.0
13		56.0	51.6	70.0	68.0	61.0	62.0	62.0	62.0
14		57.0	50.0	66.0	66.0	58.0	59.0	60.0	59.0
15		49.0	50.0	62.0	62.0	57.0	57.0	64.0	57.0
16		45.0	42.0	57.0	61.0	52.0	53.0	53.4	52.0
17		40.0	41.0	47.0	52.0	44.0	48.0	50.0	46.0
18		35.0	36.9	39.0	49.0	41.0	42.0	47.0	41.0
19		31.0	32.0	34.0	44.0	37.0	37.0	43.0	37.0
20		27.0	31.0	31.0	40.0	35.0	33.0	42.0	34.0
21		22.7	28.0	27.0	33.7	29.0	28.0	34.0	29.0
22		15.0	24.0	20.0	25.0	25.0	21.0	24.0	22.0
23		1.0	2.0	3.2	2.0	2.2	4.3	2.7	2.0
DAILY TOTAL		776.9	796.7	994.2	919.7	858.5	880.3	905.5	876.0

Weekday								
AM Peak Hour			PM Peak Hour			Daily		
In	Out	Total	In	Out	Total	In	Out	Total
64	64	128	52	52	104	892	892	1,784

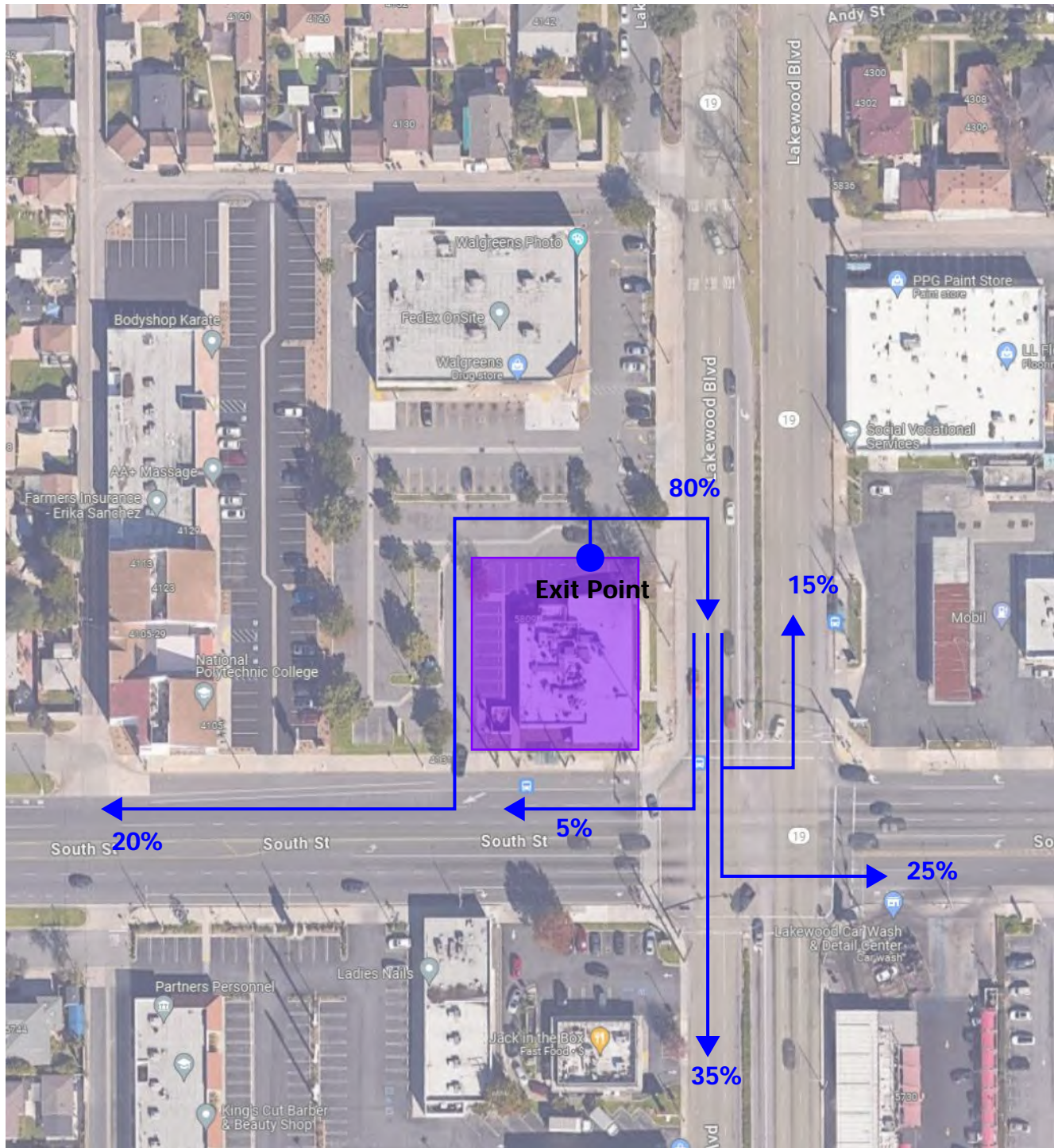
* took the maximum of the average of the 85th percentile of transactions across the reference locations during the associated peak period
 ** added 32 trips to the Daily Trip Generation Estimates to account for commuting worker traffic

APPENDIX D

Trip Distribution Patterns



IN FLOW



OUT FLOW

TECHNICAL MEMORANDUM

August 8, 2024

Project# 30388.006

To: Paul Kuykendall, AICP
City of Lakewood
5050 Clark Avenue
Lakewood, CA 90712

From: Kittelson & Associates, Inc

RE: Dutch Bros at 5809 Lakewood Boulevard – Queuing Memorandum

A 950 square foot Dutch Bros with a drive-through is proposed at 5809 Lakewood Boulevard, Lakewood, California. This technical memorandum evaluates on-site circulation, site access, and queuing scenarios for the proposed Dutch Bros coffee shop ("project").

PROJECT CONTEXT

The proposed project would replace a vacant restaurant building that shares its parking lot and site access with Walgreens. The current site has five driveways giving vehicular access – two from Lakewood Boulevard, one from South Street, and two along the alley north of Walgreens. The project site would consist of a 950-sf retail structure, three vehicle parking stalls including one ADA stall, and a vehicle drive-through served by a single service window. The drive-through has two lanes that converge at the service window and is designed to accommodate a queue of 16 vehicles at one time. It is anticipated for the project to be operational by 2025.

A detailed preliminary site plan is provided in Appendix A.

SITE PERFORMANCE ANALYSES

The existing roadway conditions and proposed site plan were assessed to determine if on-site safety or operational improvements were necessary due to an increase in traffic from the project.

Site Access and On-Site Circulation

When reviewing the proposed site access and on-site circulation, the following details were noted:

- **Parking.** The project would reconfigure the parking lot within its parcel and provide a total of 36 parking spaces. Parking would not be regularly needed by customers since it is primarily a drive-through operation. The parking spaces would be available as part of a shared lot for users of the shopping center where the project is to be located, and collectively is assumed to suffice to meet the needs of the project for employees and any walk-up customers.
- **Driveway Impacts.** Exiting queues at the south driveway along Lakewood Boulevard may disrupt on-site circulation, particularly by blocking vehicles from exiting the drive through. More than one

queued vehicle exiting the shopping center at this driveway can impede project drive through exiting vehicles, while queues over four vehicles would also interfere with drive-through entry and access. The south driveway to Lakewood Boulevard is right-in, right-out access so vehicles exiting would only need to find a gap in southbound traffic on Lakewood Boulevard. Dutch Bros should monitor – especially during peak operation periods – this driveway adjacent to their drive through exit and reroute vehicles to the north driveway on Lakewood Boulevard that also provides right-out only access to Lakewood Boulevard. A more permanent reroute of vehicles exiting the drive through could be installed if this becomes an ongoing issue.

- **Sight Distance.** Sight distance from the drive-through entrance and exit were assessed, and no anticipated issues were found. The site plan confirms that there would not be any landscaping or other installations obstructing sightlines. There is an existing tree on the east side of the drive through lane exit but it should not prohibit visibility for vehicles exiting as there is adequate throat in the exit lane that positions vehicles to have adequate monitoring of approaching cyclists, pedestrians, or vehicles in the parking aisle.
- **Drive Through Vehicle Access.** The site plan provides visibility for vehicles approaching the entrance from any direction to see other oncoming vehicles and to see the existing queue to determine the appropriate drive through lane to enter. Vehicles approaching the drive through lane from the South Street driveway would be required to make a U-turn into the drive through lane but is not anticipated to be an issue. The drive-through entrance is surrounded by parking and would not conflict with other on-site operations.
- **Adequacy of Pedestrian Facilities.** Pedestrian access to the site would include existing sidewalks, crosswalks and walkways along the project frontage that would facilitate pedestrian access. There are standard crosswalks across all legs at the Lakewood Boulevard / South Street intersection.
- **Bicycle Accessibility.** South Street has Class II bike facilities on both sides of the roadway. Bicyclists would use the drive aisles to access the project site.
- **Accessibility from Adjacent Transit Stops.** Four bus routes have stops within 500 feet of the project site. One is operated by Los Angeles Metro (Route 266) and the other three (Routes 192, 111, 112) are operated by Long Beach Transit. Riders alighting Route 266 and 192, operated by Los Angeles Metro and Long Beach Transit, respectively, have direct access to the project site. Riders alighting Routes 111 and 112, need to cross at Lakewood Boulevard / South Street to access the project site. Sidewalks are provided from the bus stops to the project site.

Drive Through Queuing

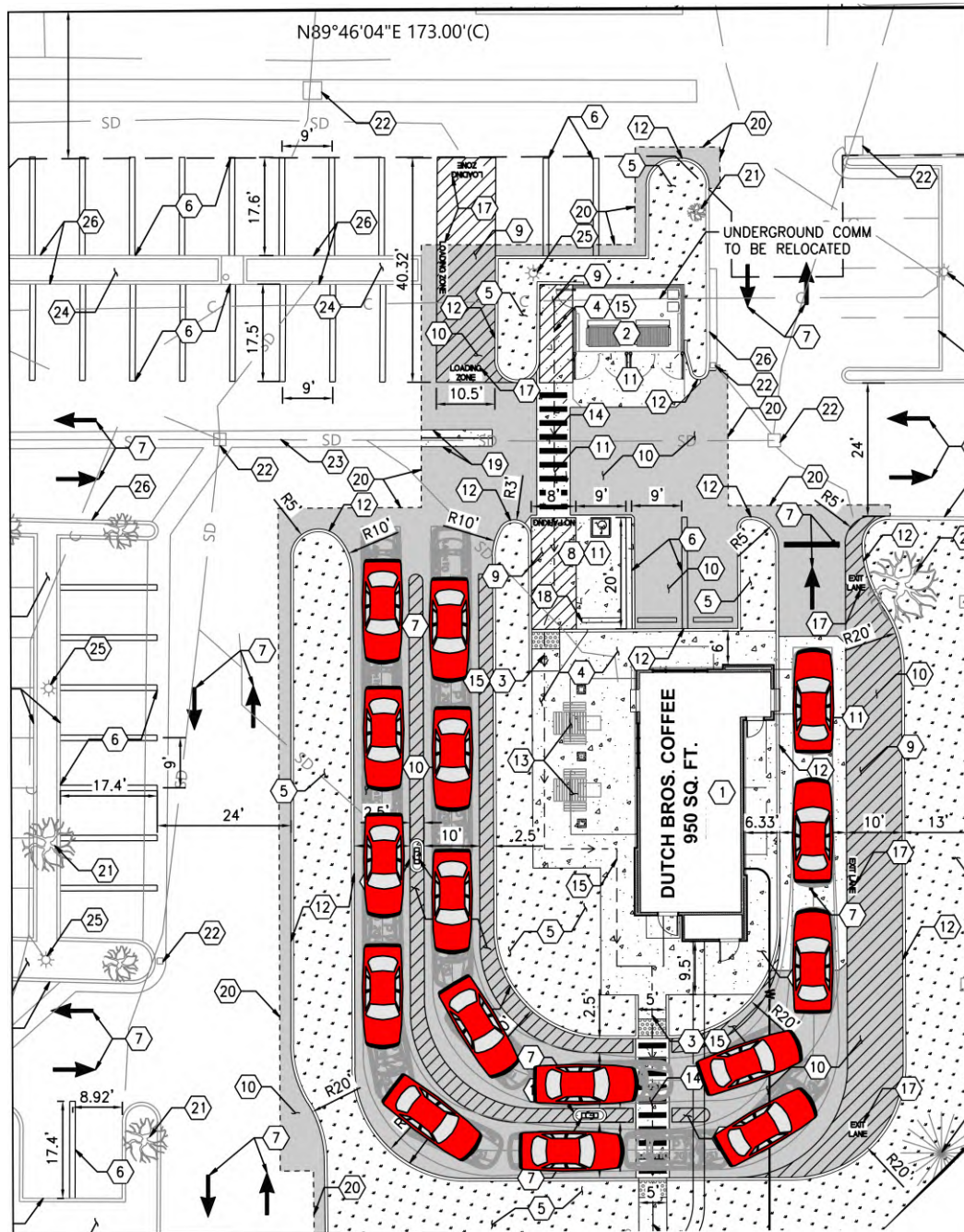
Queuing at coffee shop drive-throughs often are where potential impacts to the roadway network occur. To estimate the potential queueing conditions and how often it may affect public right-of-way, an M/M/1 queueing model was used. This model assumes that customer arrivals and service times are random and requires the average arrival and service rates as inputs. Using historical transaction data from similar Dutch Bros stores, 67 and 80 vehicles per hour were used as conservative arrival and service rates, respectively, for the proposed store.

From the model, it was determined that there is approximately a 5% probability that the number of vehicles waiting to be served will be longer than 16 vehicles, as shown in Figure 1. Since the drive-through can accommodate up to 16 vehicles, it is expected that queues would not affect the public right of way during the highest 5% of demand times. However, it is still recommended for Dutch Bros to implement

the following strategies upon reaching 75% of drive-through capacity (approximately 12 vehicles) to manage queues as efficiently and effectively as possible:

- Dutch Bros staff takes orders and payments from and makes deliveries to the queued drive-through lanes to minimize the wait time at the service window. The site plan includes a queue exit lane adjacent to the pick-up window. This is an important queue length management feature because it allows vehicles that receive their fulfilled orders prior to reaching the pick-up window to exit out of the queue early, thereby reducing the overall length of the queue.
- Dutch Bros staff can place signage to guide non-Dutch Bros customer vehicles away from the drive-through entrance and act as temporary traffic control personnel who can effectively manage queues and prevent blockage situations.
- Parking on site can be used as waiting areas for vehicles that make large orders. This is an important operations factor that provides the ability to remove vehicles with long order fulfillment times from the queue and provide a significant positive effect on the overall queue length.

It should be noted that accurately estimating total latent demand is difficult due to various factors. These factors include the location, type, convenience, and pricing of competing opportunities in the area, as well as the traffic volume on adjacent streets and the socioeconomic characteristics of the nearby population and employment areas.

Figure 1: 95th Percentile Queuing Scenario

SOURCE: KITTELSON AND ASSOCIATES (2024); SITE PLAN IS DRAWN BY BARGHAUSEN CONSULTING ENGINEERS (2024)

APPENDIX A

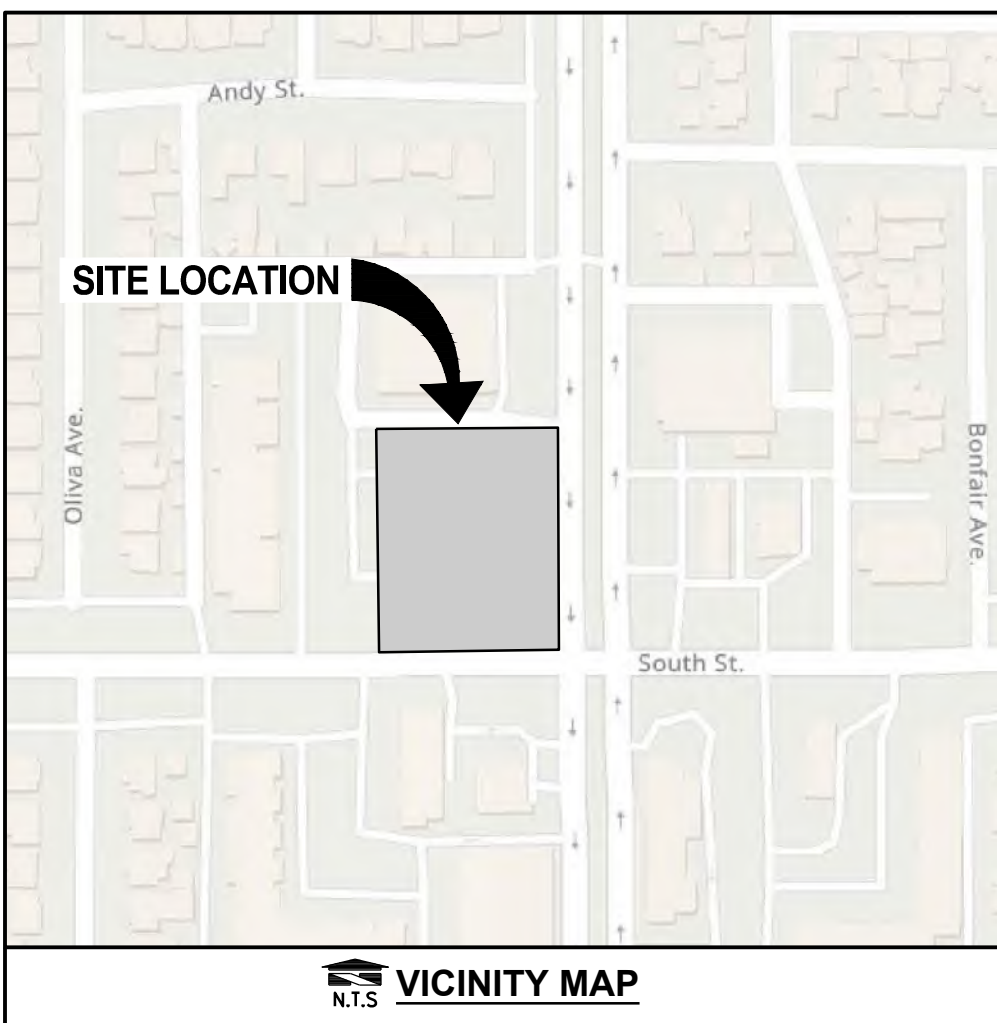
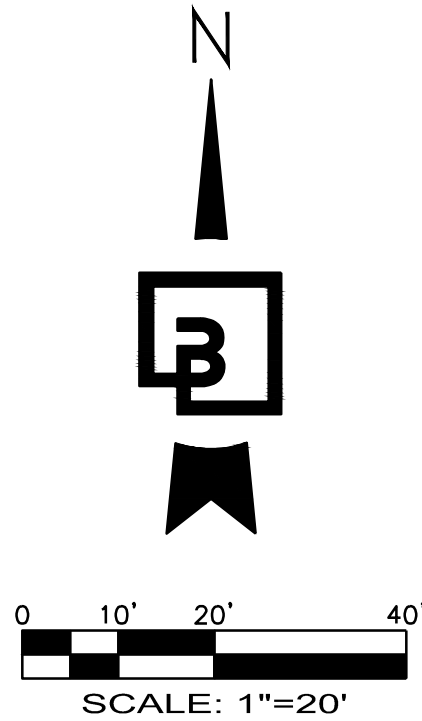
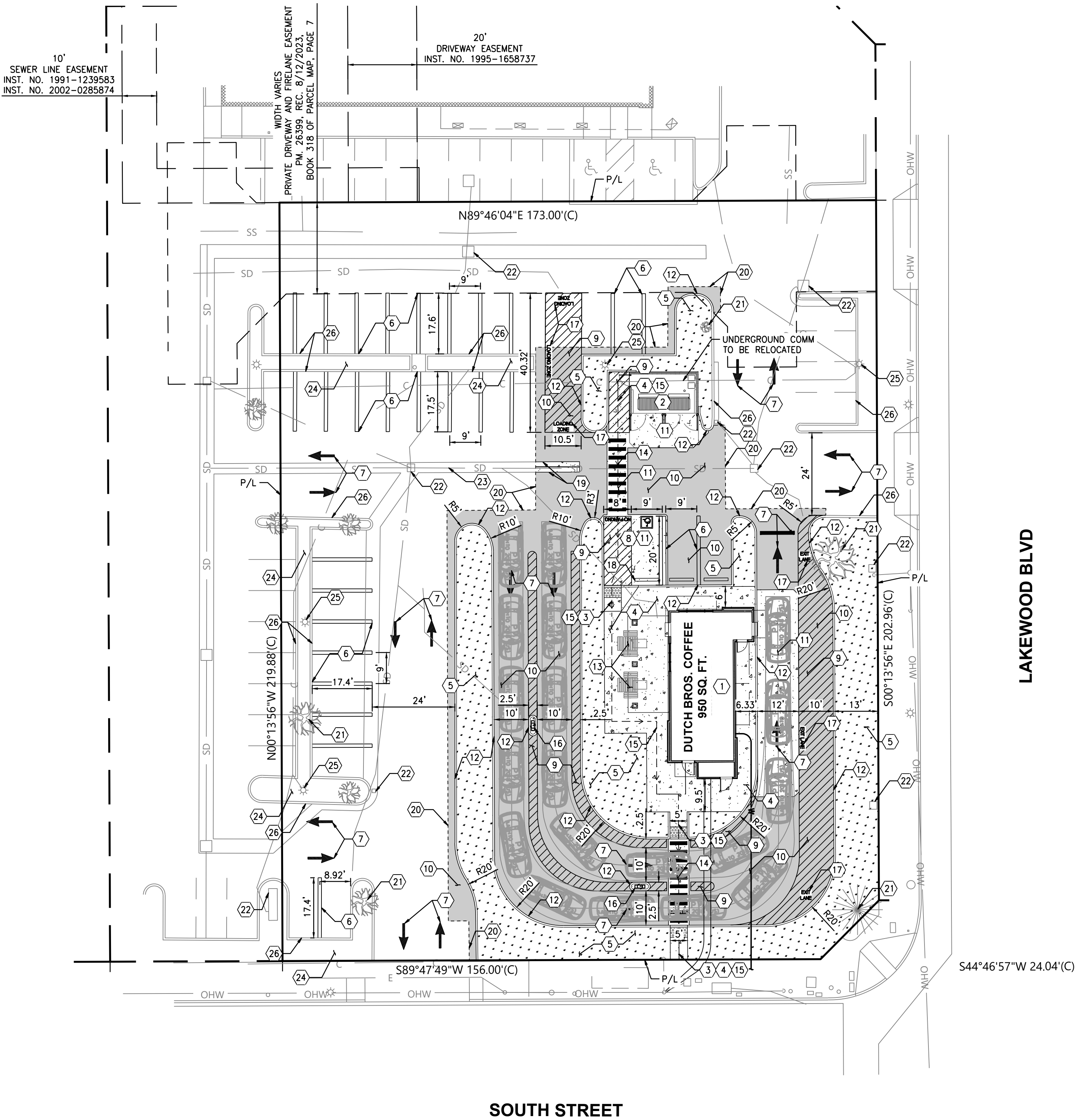
Site Plan

"The name DUTCH BROS., and all associated logos, distinctive designs, content, information, and other materials featured, displayed, contained herein, and made available by Dutch Bros., including but not limited to, the "look and feel" of the establishments and products, all text, images, colors, configurations, graphics, designs, illustrations, photographs, and pictures (collectively, the "Materials") are owned by and/or licensed by DB Franchising USA, LLC and are protected by copyright, trademark, trade dress, patent, and/or other intellectual property rights and unfair competition laws under the United States and foreign laws."



Know what's below.
Call before you dig.
Dial 811

DUTCH BROS COFFEE CA6405
5809 LAKEWOOD BLVD., LAKEWOOD, CA
PRELIMINARY SITE PLAN



CONSTRUCTION NOTES:

1. PROPOSED DUTCH BROS COFFEE STAND; REFER TO ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.
2. PROPOSED 24'X12' TRASH ENCLOSURE; REFER TO ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.
3. PROPOSED ADA RAMP.
4. PROPOSED PRIVATE CONCRETE SIDEWALK (WIDTH PER PLAN).
5. PROPOSED LANDSCAPING AREA; REFER TO LANDSCAPING PLANS FOR ADDITIONAL INFORMATION.
6. PROPOSED 4" WIDE WHITE REFLECTIVE PAINT PARKING STALL STRIPES, TYPICAL.
7. PROPOSED PAVEMENT TRAFFIC MARKINGS, TYPICAL.
8. PROPOSED ACCESSIBLE PARKING SPACE AND AISLE WITH ALL REQUIRED SIGNAGE.
9. PROPOSED PAVEMENT STRIPING: 4" WIDE, 24" O.C., 45 DEGREE ANGLE, SAFETY WHITE.
10. PROPOSED ASPHALT PAVING.
11. PROPOSED CONCRETE PAVING.
12. PROPOSED BARRIER CURB.
13. PROPOSED LOCATION OF FIXED 4 SEAT PICNIC TABLE.
14. PROPOSED CROSSWALK PAVEMENT STRIPING.
15. PROPOSED ACCESSIBLE PATH.
16. PROPOSED LOCATION SIGN.
17. PROPOSED 12" PAVEMENT LETTERING.
18. PROPOSED PRECAST WHEEL STOP, TYPICAL.
19. PROPOSED VALLEY GUTTER.
20. PROPOSED SAWCUT LINE.
21. EXISTING TREE, TYPICAL.
22. EXISTING UTILITY STRUCTURE AND ASSOCIATED UTILITIES SHOWN TO REMAIN.
23. EXISTING VALLEY GUTTER TO REMAIN.
24. EXISTING LANDSCAPING TO REMAIN.
25. EXISTING SITE LIGHT.
26. EXISTING CURB.

PROJECT DATA

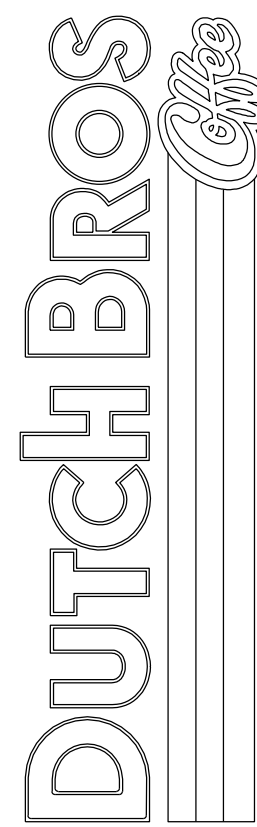
DB2550-A1			
NAME: DUTCH BROS COFFEE - LAKEWOOD, CA			
ADDRESS: 5809 LAKEWOOD BLVD., LAKEWOOD, CA 90712			
PROPOSED PARCEL AREA = 37,902 SQ. FT. (0.87 ACRES)			
PARKING SPACES:			
- PROPOSED REGULAR PARKING	=	2	
- PROPOSED ADA PARKING	=	1	
- EXISTING TO REMAIN	=	33	
- TOTAL PARKING	=	36	
QUEUEING = 19			
TRASH ENCLOSURE = 20' X 12'			
PROJECT AREA COVERAGE:			
BUILDING/CANOPY, TRASH ENCLOSURE	±1,190 SF	(6.27%)	
PARKING AND MANEUVERING:	±9,561 SF	(50.35%)	
WALKWAYS:	±2,818 SF	(14.84%)	
LANDSCAPE:	±5,419 SF	(28.54%)	
	18,988 SF	(100.0%)	

LEGEND

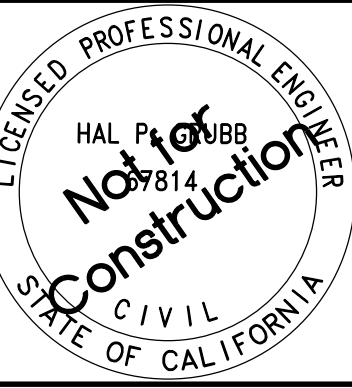
BUILDING LINE	
EXISTING CURB TO REMAIN	
PROPOSED CURB	
PROPOSED LANDSCAPING	
PROPOSED ASPHALT	
PROPOSED CONCRETE	

PRELIMINARY SITE PLAN
5809 LAKEWOOD BLVD.,
LAKEWOOD, CA

Title:

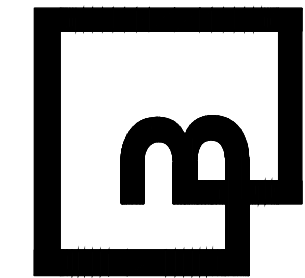


For:



Scale:		Horizontal	Vertical
1" = 20'		1" = 20'	N/A
Designed RAG		Drawn RAG	Checked MTL
Approved JAH		Date 7/15/24	

Barghausen
Consulting Engineers, Inc.
18215 72nd Avenue South
Kent, WA 98032
425.251.6222
barghausen.com



Job Number
23477

Sheet
1 OF 1
2019 DB
Franchising USA, LLC

PRELIMINARY NOT FOR CONSTRUCTION



April 18, 2025

DRB RECOMMENDATION TO PEC

Property Owner

Qing Kai Investment, LLC
Attn: Danruo Yan
9324 East Garvey Avenue, Suite O
El Monte, CA 91733
E-mail: ZkLi998@gmail.com

Tenant

Dutch Bros LLC
Attn: Gurinderjit Kehr
PO Box 1929
Grants Pass, OR 97528
E-mail: gurinderjit.kehr@dutchbros.com
Phone: (510) 807-4278

Designer/Applicant

Barghausen Consulting Engineers (BCE)
Attn: Brianna Uy or Sabrina Rushing
18215 72nd Avenue South
Kent, WA 98032
E-Mail: Buy@Barghausen.com
Phone: (425) 291-3776
E-Mail: SRushing@Barghausen.com
(DB# CA6405 – BCE #23477)

DRB #9251/ Dutch Bros Coffee – Drive-thru Restaurant

Site: 5809 Lakewood Boulevard, Lakewood, CA 90712;
AIN: 7160-019-050; (PM318-7-9 Lot 2)

(CUP #1026)

Area: .87 ac. (37,921 SF)
Zone District: C-3

Project Description (“Project”): The proposed project is for the construction and operation of a new 940 SF double lane drive-thru restaurant with a 300 SF patio, and a 240 SF trash enclosure, including demolition of an existing 6,076 SF restaurant building.

The proposed Project as herein described with related plans has been reviewed and deemed complete by the Development Review Board (“DRB”), and they hereby **RECOMMEND APPROVAL** to the Planning and Environment Commission (PEC) of Conditional Use Permit (CUP) Number 1026 to establish the land use and to allow the construction and operation of the Project, subject to the proposed conditions listed in Attachment No.1. These conditions, if adopted, shall be completed and continually operated and maintained on an ongoing basis by the applicant to the satisfaction of the Community Development Department. This action was taken by unanimous vote of a quorum of the DRB in consideration that all the required findings specified by the Lakewood Municipal Code (LMC) Section 9484.1 are being made in a positive manner supporting the development of the Project. This action is not appealable as it is an advisory recommendation only.

Lakewood

PERMITS: The applicant shall submit building plans for plan check, obtain permits to construct the approved Project, and achieve a successful final inspection of the Project construction, prior to occupancy and operation of the proposed Project. The City website located at <https://www.lakewoodca.gov/Building-Planning/Building-Planning-Process/Building-Plan-Check> has documents that will assist, including “*How to Electronically Submit Building Plans & Permits*,” “*Electronic Plan Check FAQ’s*,” and “*Part 1 - Online submittal of plans for Plan Check*.”

Should you have any further questions concerning this matter, please contact the Assistant Director of Community Development, J. Patrick McGuckian, at (562) 866-9771, extension 2302.

Sincerely,



J. Patrick McGuckian, AICP
Assistant Director of Community Development

Attachments: CEQA Determination and Conditions of Approval

Copies to: Building and Safety Division,
Public Works Department, and
Public Safety Department

ATTACHMENT

PROJECT DESCRIPTION

The proposed (“**Project**”) is for the construction and operation of a new 940 SF double lane drive-thru restaurant with a 300 SF patio, and a 240 SF trash enclosure, including demolition of an existing 6,076 SF restaurant building.

CEQA DETERMINATION

The DRB recommends that the PEC find that this Project is exempt from the State of California Environmental Quality Act (CEQA) as authorized by Public Resources Code Section 21084 and as implemented by the CEQA Guidelines (*California Code of Regulations, Title 14 Natural Resources, Division 6 - Negative Declaration Process, Chapter 3 CEQA Guidelines, Article 19 – Categorical Exemptions*), based on the following:

Infill Development: *Section 15332, Class 32: Infill development Projects.* The Project will be:

(a) Consistent with the applicable General Plan designation and all applicable general plan policies as well as the applicable zoning designation and regulations. (b) Within the city limits on a project site of no more than five acres substantially surrounded by urban uses. (c) On a site that has no value as habitat for endangered, rare, or threatened species. (d) A land use that will not result in any significant effects relating to traffic, noise, air quality, or water quality. (e) On a site that is adequately served by all required utilities and public services. **NOE:** A Notice of Exemption (NOE) may be prepared and recorded based on this finding, if requested by the applicant.

CONDITIONS OF APPROVAL

LAND USE/PROJECT APPROVAL

1. Project and Land Use Approval. The following Project and land use are **APPROVED**, including all plans and all are subject to the following development and operational conditions of approval. The proposed project is based on the information provided by the applicant without verification. The applicant is responsible to properly display property lines, easements, structures, zoning, and other improvements. Failure to provide accurate and complete information may result in revision or revocation of this approval.

The project shall be constructed and operated in compliance with the project requirements listed herein, and in the Lakewood Municipal Code. The applicant shall not deviate from the approved plans without written approval of the change from the Planning Division of the Community Development Department. The Project includes approval to:

- a) Demolish an existing 6,208 SF restaurant building.
- b) Construct and operate a new 940 SF Drive-Thru restaurant building, 240 SF trash enclosure and a 300 SF covered patio with 8 seating spaces for walk up customers on a 37,902 SF (.87 ac.) lot as an approved land use.
- c) The land uses and structures shall adhere to regulations of the zoning district, namely Lakewood Municipal Code Section 9347 et. seq. for the C3 zone district.

d) This project approval includes:

- A site plan, a roof plan, a floor plan, four-sided elevations and a demolition plan
- A parking lot plan showing areas to be slurry sealed and those to be repaved.
- Preliminary landscaping and irrigation plans.
- Architectural material plans with color scheme and elevations.
- Plans for a fully roofed and secured trash enclosure per city specifications.
- Short-term stay bike racks for customers and long-term stay bike lockers for staff.
- Exterior Seating Protection Plan.
- Circulation and way finding signage and markings.
- Commercial Signs (*Subject to separate staff review procedure*)
- Preliminary Grading Plans.
- Public Works approved Low Impact Development storm water retention system plans.

2. Conditions of Operation:a) Hours of Operation: The facility may operate 24 hours a day, seven days a week.b) Maximum customer occupancy.c) Maximum Occupancy:

- Customers maximum capacity is: 50
- Staff on largest shift is estimated to be: 10
- Maximum total occupancy of the building shall be (*staff and customers*): 60
- Customer capacity shall be 50 consisting of 38 in drive-thru, 8 seated in exterior patio area and 4 at walkup window.
- Staffing capacity is estimated to be 30 Part-Time Employees (PTE) and 4 Full Time Employees (FTE) with a maximum of 10 staff on the largest shift.

d) Menu Order Board. Any drive-thru menu board with speakers shall have adjustable volume controls so that sound generated by the drive-thru will not be audible at the closest property line. It is acceptable to have no speakers with orders taken at the takeout window or by external staff using mobile devices in the order lanes remotely.e) Drive Thru Circulation. On site traffic circulation shall be designed and operated to prevent any queuing from backing up into the offsite public right-of-way in a manner that obstructs normal traffic movements. Staff and materials shall be deployed to facilitate on-site traffic movements and to assist both the ingress to and egress from parking stalls.

- The southern driveway facing Lakewood Boulevard, near South Street shall be restricted to egress traffic only. Signs facing Lakewood should state “EXIT ONLY”
- The dual drive-thru ordering queue shall have a lane merge sign at the merge point stating “YIELD – ALTERNATE LANES” or similar.

f) Operational Impacts. No activity associated with the construction, maintenance, or operation of the approved Project and land use shall be conducted in such a manner that it generates excessive noise, glare, odor, dust, or ground-borne vibration that impacts adjacent properties. Stationary-sourced noise generated from equipment and uses on the site shall not exceed 65 dB(A) at any adjacent property line.2. Permits. The applicant shall submit building plans, obtain permits, and construct the Project as described above and in accordance with the PEC approved plans, which are on file with the Community Development Department, and obtain successful final inspections. A licensed contractor is required for most permits to be issued. Requests for building permit inspections should be made two or more business days in advance of the inspection. The site improvements shall be constructed and operated in accordance with the following reviews and documents:

- a) Planning Actions. The PEC approved plans, these conditions of approval, other related drawings, and documents (*All as on file with the Community Development Department*).
 - b) Los Angeles County Building and Safety. Comply with all California building, and construction codes as currently and most recently adopted by the City of Lakewood and the County of Los Angeles, including but not limited to building, structural, mechanical, electrical, plumbing, Title 24, and the green codes. (*Property owner should receive and retain a copy of the city-signed final inspection card prior to contractor's final payment.*)
 - c) Los Angeles County Fire. Comply with any required County Fire plan reviews and inspections. This is an independent plan review and needs to be submitted separately.
 - d) Los Angeles County Public Health. Comply with any required County Public Health plan reviews and inspections. This is an independent plan review from the Building and Safety review and needs to be submitted separately. Any food storage/preparation uses shall obtain approval from LA County Public Health.
 - e) Community Development (CD) – Landscaping. Prior to final inspection or occupancy:
 - i. The landscape planting, revitalization and replanting program, and the landscape irrigation system shall be completed in accordance with the approved plans and as approved by planning staff in coordination with the DRB Landscape Expert. According to the site plan the landscaped area is 5,419 SF.
 - ii. A Landscape Documentation Packet (LDP) shall be submitted to and approved by planning staff to implement the Water Conservation in Landscaping regulations if the landscape area exceeds 500 SF.
 - f) Lakewood Public Works (PW). Encroachment permits, construction debris hauling and three stream waste collection programs, industrial waste permits, and any required stormwater LID system shall be reviewed/approved by the PW Department.
3. Exterior Light Fixtures. Exterior lights shall comply with LMC Section 9492.I, which requires exterior lighting be angled and shielded in a manner so as not to reflect direct or indirect light upon abutting or adjacent properties or create glare for traffic operations, with a maximum light spill of 0.5-foot candles at property line and grade level.
 4. Compliance. All existing structures shall be properly permitted and sited in compliance with the Lakewood Municipal Code. Any non-complying structures shall either be removed or properly permitted, prior to final inspection of the requested improvements.
 5. Materials. The applicant shall coordinate and/or match throughout all architectural elements, including the size, style and shape of roof overhangs, rafters, and fascia. All windows and doors shall match in terms of trim and style on each elevation. All roofing materials shall match, and all exterior materials, textures and colors shall blend. The applicant shall install any materials specifications that are included in the approved plans.
 6. Paint. The applicant shall paint the new building and other structures as specified in the plans. Exterior portions of any structures that have cracked, faded, stained, or peeled paint shall be repainted in the future. Property Owner or designee shall continuously maintain/repair all structural exteriors in a clean aesthetic manner, including prompt removal of graffiti.
 7. Signage and Façade Preparation. All exterior signage is required to be reviewed and approved by the Planning staff and shall be submitted as a separate application. Prior to wall sign installation, the wall façade behind a proposed wall sign shall be properly patched and fully repainted to be one continuous color across the full surface of the wall. There shall be no visible shadowing or silhouette of any previous sign. The sign installer is required to obtain a final inspection of the installed sign. (*The applicant should receive a city-signed final inspection, prior to final sign contractor payment.*)

8. Address Numbering (Public Safety/CD). Address numbers for commercial buildings shall be 6" or taller and clearly visible to emergency staff from the street. A painted 3-foot-tall address number on the roof is recommended for aerial viewing by the Sheriff's helicopter.
9. Storage Containers (Community Development). There shall be no shipping containers placed on the site as a source of temporary storage for goods without a temporary use permit issued by the Community Development Department. Any containers allowed by such a permit shall be removed in accordance with the time limits imposed by the temporary use permit. Any use of such containers used for the storage of Project construction materials and tools shall be removed prior to final inspection of the Project. Any permanent use of such containers shall be approved as part of this Project approval or revision thereto and installed in accordance with the Building Codes for a permanent structure.
10. Compliance. All existing structures shall be properly permitted and sited in compliance with the Lakewood Municipal Code. Any non-complying structures shall either be removed or properly permitted, prior to final inspection of the Project.

PARKING AND CIRCULATION. *(Community Development)*

11. Vehicle and Bicycle Parking. City specifications shall be implemented for doubled striped parking spaces and for other striping, signage, and traffic circulation direction markings and signage, including stop signs, stop limit striping, pedestrian crossings, and left and right turn arrows. The proposed project shares parking and parking lot maintenance with the adjoining lot (*currently occupied with Walgreens*), subject to a reciprocal access and maintenance agreement. All parking with required markings shall comply with the CalGreen Code (CG) as specified in the California Code of Regulations Title 24, Part 11, effective January 1, 2023. The parking specifications of LMC Section 9490 shall be imposed, as follows:

Required/Provided Parking Spaces:

Parking demand by land use per LMC Section 9490 U.2.:

Fast Food Restaurant [1/3 seats (*8 seats*); 1/100SF (*950 SF*); Min. 5)

Total Required **10**

Parking Spaces

Provided Parking Spaces Allocated by Type:

- Standard Spaces (9'w x 20'd) 35
- Parallel Parking Spaces (9'w x 24'd) 00
- Compact Spaces (8'w x 16'd) Allowed Max 40% of 35 provided =14 Used: 00
- Motorcycle Parking (4'w x 8'd on cement) 00
- Pickup/Delivery spaces (online delivery) Allowed above required 00
- Regulated Parking provided (*See below - ADA/EVSE*) 1

Total Vehicle Parking Provided **36**

Parking Analysis:

- Surplus/Shortfall (*36 Provided – 10 Required*) +26
- Trash Enclosure Credits (*1 sp. per upgraded Trash Enclosure = 1*) – Used = 00
- **Total Surplus/Shortfall** (*Subtract credits from required*) +26
- **AB2097** Total spaces waived if ½ mile to major transit stop of 2 bus lines 00
- Total Balance** (*Surplus/Shortfall*) **+26**

Regulated Parking Spaces:

ADA - Accessible Spaces *(Based on 10 Required Spaces)*

	<u>Required</u>	<u>Provided</u>
• EVSE Van Accessible (9'+8'w x 20'd wired)** <i>(1st of 6 ADA required)</i>	1	1
• Van Accessible (9'+8'w x 20'd) <i>(1st of 6 ADA required)</i>	0	0
• EVSE Standard Accessible (9'+5'w x 20'd wired) **	0	1
• Standard Accessible (9'+5'w x 20'd) <i>(1/40 required up to 160)</i>	0	0
• EVSE Ambulatory (10'x 20' wired) <i>(1 over 25 EVC required) **</i>	0	0
Total Accessible Regulated:	1	1

CalGreen Code (CG) Vehicles Parking Spaces *(Based on 36 Provided Spaces)*

Per Table CG5.106.5.3.1. *EV Capable = Panel space available with empty conduit installed in place and a termination box. EV Ready is fully operational with one or more EVSE in an EVCS (Electrical Vehicle Charging Station.)*

*EVSE = Electric Vehicle Supply Equipment **(Not required w/ less than 25,000 SF of new construction.)***

(If required 5 spaces = 1 EV capable/1 EVSE)

Loading Zones (LZ) *(City Standard is 10'w x 25'd – one per building)*

• Truck Loading Spaces (10'w x 25'd x 14'h) <i>(1 per bldg.)</i>	1	1
• Dock Loading Spaces (10'w x 60'd) <i>(as needed)</i>	0	0
Total Loading Spaces Provided	1	1

Bicycle Storage *(Require 5% of Required Parking for both Long & Short Term)*

• Short-Term External Bike Storage* <i>(5% of 10 Required parking spaces)</i>	1	1
• Long-Term Internal Bike Storage* lockers/rooms <i>(staff 5%)</i>	1	1
Total Bike Storage Provided:		2

*(*Refers to total bikes stored - Bike racks/lockers may store more than 1 bike)*

14. **Loading Zone.** The Loading Zone shall be marked on the pavement as a “LOADING ZONE” with cross hatched lines to indicate that this is reserved for loading and unloading. The loading zone shall be a minimum unobstructed area that is 10' wide and 25' wide with a clear and unobstructed height that is a minimum 14'. There shall be no storage or intruding vegetation in this area. It shall be unobstructed and clear at all times.
15. **ADA Driveway Visibility.** Any ADA paths-of-travel that either crosses or is immediately adjacent to any street or driveway, shall be outlined with a 3" or wider white stripe on each side of the blue striped pathway to increase the visibility of the crosswalk or pathway.
16. **Restriping and Slurry Seal.** The parking lot shall be repaved or slurry sealed, as necessary, refreshing markings, traffic signage, and refreshing double-striped parking spaces per city standards, as determined necessary by the Community Development Department.
17. **Parking Lot Markings/Signs and Striping.** Prior to final inspection and occupancy of the project, the parking lot shall be inspected and approved by the Planning staff for proper maintenance and driver visibility. The evaluation shall include all parking spaces (*number & dimensions*), double striping parking spaces, wheel stops, driveways widths, including:
 - a) The slurry seal, repave and restripe of the parking lot per the approved plans.
 - b) Parking spaces conform to approved plans (*number, type and dimensions.*)
 - c) Installation of on-site traffic markings and signage (*e.g., stop signs, stop limits, pedestrian crossings, and turn arrows.*)
 - d) The double entry drive aisle merge point shall add a sign that states “YEILD - ALTERNATE LANES ”

18. Fire Lane (*LA Co. Fire*). A fire lane shall be maintained in a clear and unobstructed manner at all times. there shall be no temporary or permanent storage in this area. In addition, there shall be no storage of combustible materials within eight (8) feet of any property line.
19. EV Capable Parking Spaces. The property owner is encouraged to address the increasing demand for such spaces with the addition of some electrical vehicle charging stations.
20. Exterior Seating Vehicle Protection. Any current or future exterior seating area (*e.g., patio*) shall be protected with a vehicle protection barrier (*e.g., steel post bollards spaced at 5.5-foot or less centers*) This is required for those portions of the patio adjacent to any street, driveway, or parking area to provide protection for those seated in the external patio from potential mishaps and intrusion by vehicles parked or traveling in adjacent areas. This detail shall be shown on the site plan with construction plans submitted to Plan Check.

WASTE COLLECTION AND DISPOSAL (*Public Works*)

21. Construction Debris. Any construction activities that involve hauling away demolition and construction debris shall be approved by the Public Works Department through the Green Halo disposal monitoring system. The city has an authorized franchised hauler, it is EDCO.
22. Self –Hauling. Any debris self-hauling (*i.e., not by EDCO*) requires preapproval by the City and shall be hauled to a city approved disposal facility with mandatory submittal of the waste haul receipts to the city to satisfy state regulations. Any failure to do so shall result in significant fines and penalties, including possible revocation of building permits and business licenses of the project contractor and any subcontractor involved in any activity related to such hauling. Review and approval by Public Works are required for any self-haul operation. These services shall be provided at no additional expense to the property and shall meet the city reporting requirements as specified in the Lakewood Municipal Code Sections 5356 and 5357. The information shall include the hauler name, the city approved disposal facility to which it is being hauled, and the frequency and estimated volume of such hauled waste material. The self-hauler shall be required to provide waste haul receipts to the City from the disposal facility and failure to do so shall result in penalties and/or license revocation.
23. Waste Collection Program (WCP). The construction debris collection program, solid waste/recycling/organic waste collection program, and trash enclosure standards for the restaurant shall comply with current City and State requirements. Prior to final inspection and/or occupancy of the proposed land use, the property owner or their representative shall prepare and obtain approval of a written comprehensive Waste Collection Program (“WCP”) for the proposed land use that is acceptable to the satisfaction of both the City Public Works Department and EDCO, the City’s franchised solid waste hauler. The WCP shall include:
 - a) Capacity. Types, size, and location of bins, as well as the frequency of pickup to satisfy volume shall be listed. There shall be sufficient capacity and storage areas for all required forms of collection. This includes all three normal waste streams of 1) solid waste, 2) required recycling (*split bin is possible*), and 3) required organic waste (*food and green landscape waste*.)
 - b) Alternatives. The use of a trash compactor, cardboard bailer, or other alternative on site storage of recycling waste shall be identified and approved in the WCP. Any self-hauling operation shall be listed in the WCP and requires city PW approval prior to hauling.
 - c) Location. All trash bins shall either be located inside the building OR in an exterior fully secured, roofed, and lockable trash enclosure, whose design is approved by both the City Community Development and Public Works Departments in coordination with EDCO. There shall be no unscreened and/or unsecured exterior storage of waste materials.

- d) Trash Enclosure Design. Trash enclosure(s) shall be constructed to comply with current city guidelines for a fully secured trash enclosure that has provisions for all three waste streams (*solids, recycling, and organic food/green waste*) as follows:
 - i. Walls shall be constructed of solid materials (*e.g., CMU block or other approved.*)
 - ii. Rainproof roof is tall enough to accommodate fully opening the top of the bins, (*Minimum 8' tall for 3 cu. yd.*). Enclosure floor shall not drain to storm drain.
 - iii. Welded wire mesh between the roof and wall with no gap greater than two inches,
 - iv. Walls shall be constructed of solid materials (*e.g., CMU block*) or other approved materials, with a lockable entry door.
 - v. Wheel stops or bollards shall be installed inside perimeter to prevent wall damage,
 - vi. Illumination of the trash enclosure area is recommended to reduce vandalism.
- e) Disposal Vehicle Access Design. EDCO shall approve the disposal vehicle access design.
 - i. Minimum 42-foot turn radius for any turns.
 - ii. Maximum 100-foot backout drive.
 - iii. Minimum 20-foot-high unobstructed clearance in front of enclosure for lifting.
 - iv. Minimum 10' x 10' reinforced cement pad in front of the enclosure for bin operations, capable of supporting 20-tons of stationary weight.
- f) Maintenance. Any trash enclosure is required to be continuously maintained by the property owner/operator/tenant in a satisfactory manner with all trash stored inside the authorized collection devices. No spillover or other external storage of debris or waste is allowed. Employees shall be instructed not to overfill bins, keep the trash enclosure doors closed and always locked to prevent unauthorized dumping or scavenging.
- g) Constant Access. Access to the trash enclosure shall be maintained at all times and not be blocked by vehicles, storage or other obstructions.
- h) Parking Lot Litter. Parking lot areas shall be maintained daily and kept litter free. There shall be a minimum of one trash barrel and one recycling bin (*Min. 20 gallons each per 100 spaces*). The trash barrels shall be lined with heavy-duty plastic trash bags that are monitored daily and emptied when full. Solid and Recyclable waste shall be separated. The trash barrel near the patio satisfies this requirement.

GRADING AND STORMWATER /LID (Public Works)

- 24. Stormwater Protection. Any construction or operational activities that could potentially result in any water leaving the site as drainage shall be coordinated with the Lakewood Public Works Department to ensure proper procedures have been implemented.
- 25. Grading Operations. Grading shall be completed in accordance with the approved grading plans and best management practices (BMP) in terms of prevention of air-borne (dust) or water-borne erosion. This shall include the following:
 - a) The grading operator shall suspend grading activities when winds exceed 25 MPH.
 - b) Water trucks shall periodically spray disturbed soil to keep it moist and reduce dust.
 - c) Rumble grates/plates shall be installed at site entrances to reduce trucks tracking out.
 - d) Soil retention devices (swaddles/silt fences) shall be installed around the perimeter.
 - e) In the unlikely event that an archaeological, paleontological or a California Native American tribal cultural resource is discovered, the grading operations shall be suspended. Respectively, a qualified archaeologist, paleontologist or Native American Cultural resource specialist shall be retained to examine and determine a course of action. In the event human remains are discovered, the coroner shall be notified, as well as the appropriate tribal organization, if determined to be of Native American origin.

26. Stormwater - LID Plan Review Procedures/Fees.

a) Procedure:

- i. Prior to planning approval of the project, by the PEC, the City Public Works Department Director or designee (PW Director) shall determine if a Low Impact Development (LID) Plan is required and if so, the applicant shall indicate the proposed location on the site plan and submit the preliminary LID design to PW.
- ii. Prior to submitting plans to the Building and Safety plan check system, the PW Director in conjunction with John L. Hunter and Associates shall approve the installation details, final design, and location of the proposed LID system.
- iii. Prior to issuance of grading or other construction permits, the applicant shall obtain in writing approval from the Lakewood Public Works Department that the FINAL project designs and devices for the proposed stormwater retention/ detention/ discharge/ clarification systems (“LID Plan”) are acceptable and in compliance with the Lakewood Low-Impact Development (LID) Handbook and the current Lakewood MS4 (Municipal Separate Stormwater Sewer Systems) Permit issued by the Los Angeles Regional Water Quality Control Board (LA - RWQMB.)
- iv. Prior to final inspection and occupancy of the Project the property owner shall obtain in writing approval from the City Public Works Department that the LID Plan has been installed to the satisfaction of the Public Works Department Director (“PW Director”), including agreements and service arrangements pertaining to the ongoing maintenance and periodic inspections of the LID Plan devices.

b) Fees. The LID Plan review, including design adequacy determinations, installation and operational compliance inspections, are conducted by an outside contracted consultant on behalf of the City of Lakewood. These reviews are conducted under the direction of, and with the final approval and concurrence of the PW Director. The applicant is responsible for all expenses involved in these reviews and inspections. The applicant is required to deposit sufficient funds to compensate for these reviews and inspections, prior to such reviews/inspections being conducted as determined necessary by the PW Director. The applicant shall deposit additional funds periodically as needed, prior to completion of each review, prior to the required inspections of construction and the on-going schedule of operational maintenance.

c) LID Plan Contents. The LID plan shall include and address the following:

- **Site Design:** The proposed stormwater facilities that are required to meet the City of Lakewood Low-Impact Development (LID) Handbook requirements.
- **Grading Plans:** Include the manufacturer, layout and details of the approved LID system the grading plans. Final or precise grading plans shall be provided for review and approval, prior to the issuance of any construction permits.
- **MCA:** The applicant shall complete, sign and date the Master Covenant & Agreement (MCA). The Lakewood Public Works Department shall provide the form. On this form, the applicant shall provide the make, model, and capacity of all proposed LID BMP(s). Attached to the form shall be:
 - 1) O & M Plan – The Operations and Maintenance (O&M) Plan.
 - 2) LID Site Plan (approved version), and
 - 3) Project Owner’s Certification (signed). The MCA shall be notarized and recorded (w/attachments) at the County Recorder’s Office. A copy of the recorded MCA package shall be provided to the Public Works Department, prior to issuance of construction permits.

LANDSCAPING

27. Landscaping and Irrigation Plans. The preliminary landscaping and irrigation plans have been reviewed and found acceptable by the DRB Landscape Expert. The final plans shall have a Plant Material Key, identifying all plant material, with symbols associated with each variety and location on the plans. The container size for each plant and tree species shall be specified, along the number and location of each to be planted.
28. Landscape Installation. The approved landscape and irrigation plans, including the required modifications, shall be installed to the satisfaction of the Community Development Director or designee, prior to final inspection and/or occupancy/operation of the Project, as follows
- a) Water Conservation. The plans shall use plants and irrigation system components that are water conserving and that comply with the city's "Water Conservation in Landscape" ordinance. Hydro zones shall be created for plants with similar watering demands.
 - b) LDP. A Landscape Documentation Packet (LDP) shall be submitted to and approved by the Planning staff if there is over 500 SF of landscaping. The LDP contains a Water Conservation in Landscaping application, Water Efficient Landscape Worksheet, soil management requirements, and the design of the proposed landscaping, and irrigation.
 - c) LDP Completion. Upon completion, the applicant shall submit a Certificate of Completion with any modifications, the landscape and irrigation maintenance schedule, and any results from either an Irrigation Audit/Survey or Irrigation Water Use Analysis.
 - d) Plant Selection. The DRB recommends and encourages the property owner to choose new plant materials that are water conserving, require lower maintenance, and are not aggressive with the capability of damaging hardscape or adjacent structures with root growth or tree limb breakage. Protection or restoration of native species and natural vegetation is encouraged, and the use of invasive/noxious plants is discouraged.
 - e) Planters. The planters adjacent to turf shall have a concrete mow strip a minimum of 6 inches wide and 6 inches deep or be a raised planter. Minimum planter dimensions shall be three feet in both length and width.
 - f) Mulch. Mulch shall be applied at the rate of four cubic yards per 1,000 SF of landscaped area with a minimum 3" deep layer of mulch.
 - g) Parking Lot Landscaping. The parking lot shall be landscaped in accordance with Section 9492.P of the Lakewood Municipal Code.
 - h) Utility Box Screening. Any utilities including transformers, gas meters, and backflow devices shall, when possible, be painted forest green and screened with landscaping. These shall not be readily visible to the public, especially from the street.
 - i) Existing Landscape. Existing landscaping that is to be retained shall be revitalized after construction activities are completed. All planters shall be weed free, all planting shall be trimmed and properly shaped in aesthetically pleasing manner within the subject lot.
 - j) New Landscaping. The landscaping plans shall conform to the following standards:
 - 1) Ground cover. New ground cover shall be planted on a maximum of 12" on center and shall achieve 90% coverage within two years.
 - 2) Shrubs/Hedge. New shrubs shall be a minimum of five gallons in size, planted in groups of three with no more than five-foot centers.
 - 3) Trees. New trees shall be a minimum of 15 gallons in size with multiple branches and a main trunk diameter that is 1" or greater measured at three feet above soil level.

SITE SECURITY

29. Facility Operations. The facility shall be well lit, clean, and organized to create a safe environment for customers. Sufficient lighting on the exterior of the premises shall be provided, including adjacent public sidewalks and parking lots. There shall be no storage of goods that obstruct or blocks access to any doorway.
30. Public Safety and Security Plan. The property owner and/or property management company on behalf of the property owner and the restaurant operator shall prepare, submit, and obtain approval of a Public Safety and Security Plan (PSSP) to the satisfaction of the Lakewood Public Safety Department (PSD), prior to issuance of building permits for the Project. Any required installations shall be completed to the satisfaction of the PSD prior to final inspection and/or occupancy of the Project.

The PSD shall review the PSSP in coordination with the Los Angeles County Sheriff's Department (Sheriff's Department). Store management shall establish an on-going liaison relationship with members of the Sheriff's Department and the Lakewood PSD staff, so that communication can be timely and effective when problems occur. This relationship shall include providing the Sheriff's Department and Lakewood PSD with both the shopping center security protocols and any individual tenant security protocols.

The PSSP shall include:

- a) Letter of Agency. The retail store tenant shall sign a Letter of Agency with the Sheriff's Department to authorize on-site enforcement for loitering, trespassing, vandalism, etc.
- b) Security Systems. A state-of-the-art security system shall be installed for both the interior and exterior store operations, which will combine motion detectors, contacts at doors, audio alarm, and a 24-hour, closed circuit video camera surveillance system. This system shall include 1) video cameras with audio capabilities, 2) Color monitors mounted in view of customers 3) Additional monitors in the back rooms, 4) alarm /panic systems shall be equipped with both fixed and remote devices and 5) required secured storage of video data for a minimum of 30 days or more, preferably monitored and stored at an off-site location.
- c) Surveillance Camera System. A state-of-the-art 24-hour, closed circuit-colored video cameras with audio capabilities shall be designed and installed for both the store interior and the exterior. A camera floor plan shall be prepared to illustrate the internal surveillance camera locations and area of coverage, and a site plan shall be prepared to illustrate the same for external operations. Both shall identify locations and areas of camera coverage and shall be approved by the PSD prior to installation. These both shall be installed and operational prior to final inspection of the Project. The security camera system shall include motion detector cameras that have pan and zoom capability with sufficient resolution for facial and license plate recognition to assist with criminal investigations. The system design should meet or exceed the following specifications:
 - Resolution of 1080p (*1920 x 1080 pixels*) at 30 FPS (frames per second)
 - Recognizable field of view (FOV) shall be a minimum 90 degrees or wider.
 - Recognizable video capture with IR Lighting under 2 lux lighting conditions.
 - Capture recognizable facial and license plate video at a minimum of 15 feet.
 - Include motion detector cameras with IVS (Intelligent Video Surveillance.)
 - Include cameras with pan and zoom capabilities.
 - Camera enclosures shall have IP ratings above 3 for both projectiles/dust and water.

- The storage medium shall be secured and not easily removed by store employees under duress or by unauthorized persons.
 - Data from security cameras shall be stored for a minimum of 30 days and employees shall provide such data to the Sheriff's Department when requested.
 - If possible, the security system shall be remotely monitored with 30 days' storage of backup data at an offsite location.
- d) POS View. Facility windows and doors shall continuously maintain a clear view from the street or adjacent driveways to point-of-sale (POS) cash register(s), if any. Clear windows and doors shall not be covered with advertising, signage, decals, etc. except those required by state and local laws. This is a safety requirement to allow security and sheriff patrols to have a clear and unobstructed direct line of sight from their vehicles to the POS store clerk, the interior of the store, the cash register area, and all persons in and around such counter space to enhance public safety. Any judgment regarding an "obstruction" shall be at the discretion of City PSD and/or Sheriff's Department's.
- e) Site Lighting. The parking lot, trash enclosures and loading dock areas shall be maintained with sufficient lighting to illuminate the appearance and conduct of all persons in the parking areas and adjacent public sidewalks. There shall be sufficient lighting around the exterior perimeter of the premises to illuminate the areas sufficiently for security camera resolution and to allow both facial and license plate recognition. However, the lighting shall not exceed .5 foot-candles at the property line.
- f) Parking Lot Signage. "NO LOITERING" signs shall be placed and remain clearly posted around the business, and any loitering should be strictly monitored and aggressively discouraged by management, including homeless encampments. In addition, the applicant shall be required to post permanent signage in a well-lit area stating "NO OVERNIGHT PARKING" along with citations of appropriate codes to facilitate towing and vehicle removal. The Sheriff's Department shall approve the wording prior to installation.
- g) Service Doors. Any service doors shall be equipped on the inside with an automatic locking and alarm device. These doors shall always be closed, except for deliveries.
- h) Facility Operations. The facility shall be well lit, clean, and organized to create a safe environment for staff and customers. Sufficient lighting of the exterior of the premises shall be provided, including adjacent public sidewalks and parking lots. There shall be no storage of goods that obstruct either ingress or egress to any doorway.

MAINTENANCE

31. General Maintenance. Commercial property owners are required to always maintain their Commercial buildings and surrounding landscaping properly in an aesthetically pleasing manner and in compliance with the Lakewood Municipal Code (*LMC 4323 and 4221.1*.) This means proper and continuous maintenance of all landscaping and structures including all roofs, painting, walls/fences, ADA paths-of travel, parking lot surface, striping and on-site circulation markings and signage.
- a) ADA markings that are adjacent to or cross over driveways shall be outlined with a three inch (3") white line on the outside of the blue ADA border stripe to increase visibility.
 - b) Any cracked, faded, stained, or peeling surface markings shall be repaired and repainted.
 - c) All exterior surfaces shall be regularly repainted to provide a new appearance.
 - d) There shall be no dead, diseased, missing, or overgrown vegetation. Vegetation shall be properly trimmed in proportion to adjoining structures and shall not encroach over property lines or onto or into public sidewalks or roadways.

- e) Landscaping shall be revitalized periodically.
 - f) Drought tolerant plants do require trimming.
 - g) All ground surfaces shall be covered with either structures, hardscape, landscape or mulch that is a minimum 3" in depth.
32. Graffiti and Stickers. The property owner is responsible for removal of all graffiti and stickers in a timely manner, no later than ten (10) calendar days from the discovery and/or notice thereof from any source. Any paint-over repair of graffiti shall cover the entire area from top to bottom and blend to the adjacent surface with no indication of a patchwork cover up.

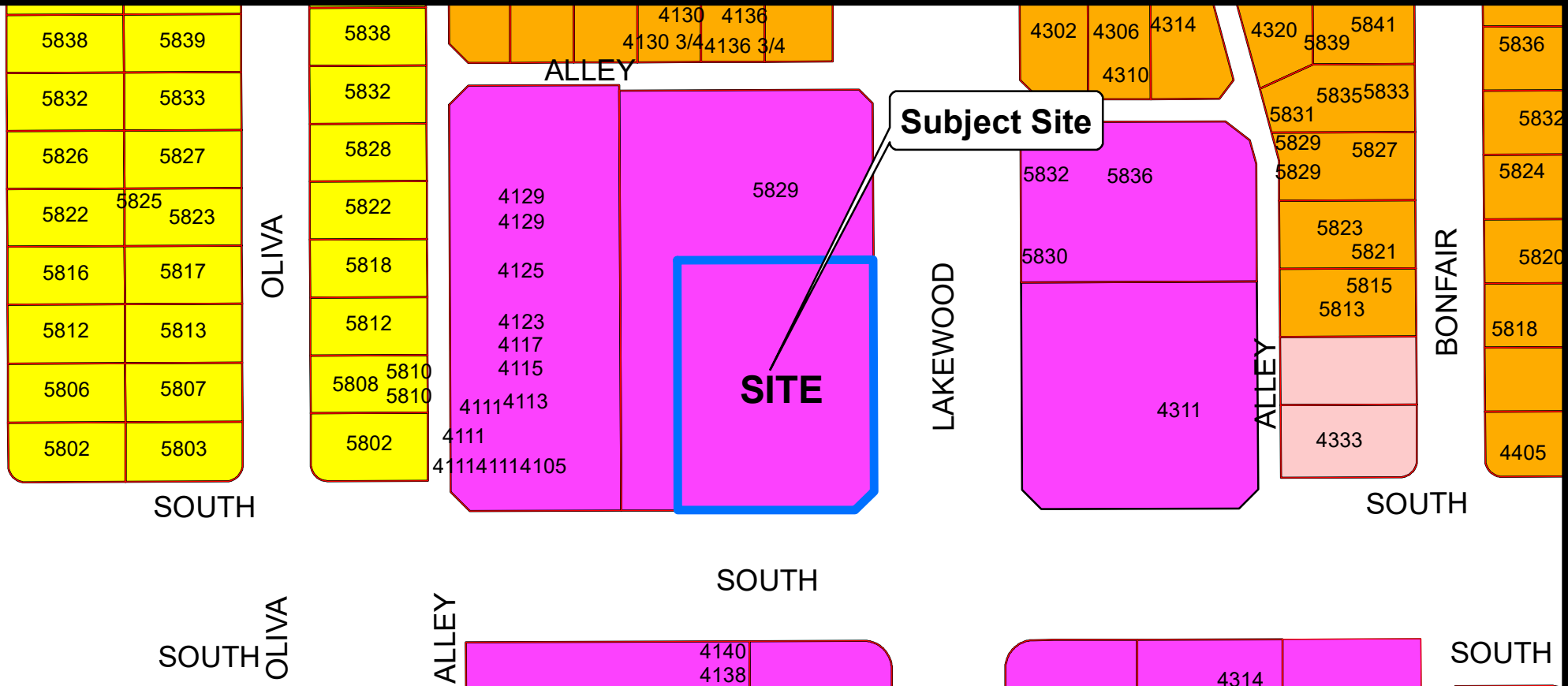
ADMINISTRATIVE STANDARDS

33. Business Licenses. All contractors, including subcontractors, sign installers, and tenants shall have properly issued city business licenses, as confirmed prior to each final inspection.
34. Notice of Appeal Rights. This PEC decision shall be final and conclusive, and effective five (5) City business days after giving of notice thereof via e-mail or the United States Postal Service to the applicant, the property owner and any other person requesting notice, unless within that five (5) City business days an appeal in writing is properly filed (*with appropriate fees*) with the City Clerk by the applicant, property owner, any protestant, or any member of the City Council.

ADVISORY NOTES. *The DRB advises that:*

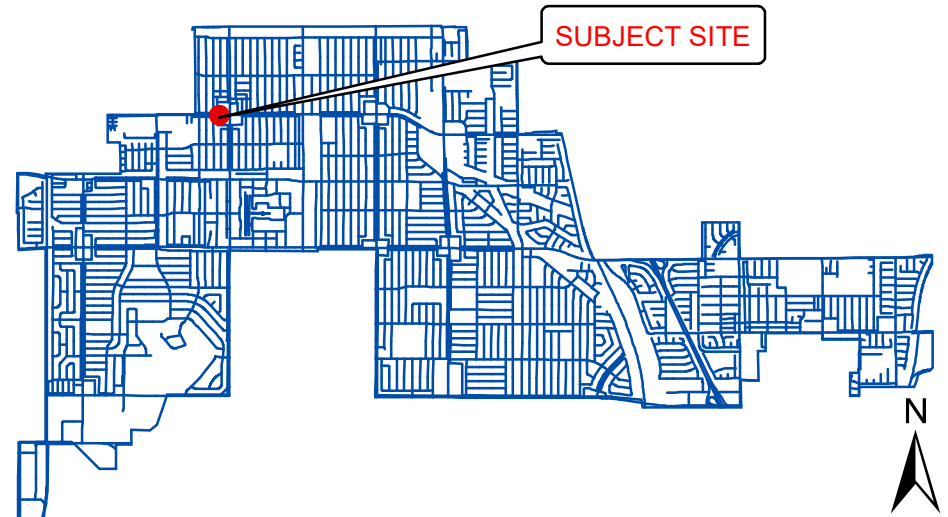
- Construction Debris. *Any construction activities that involve hauling away demolition or construction debris needs to be coordinated with and approved by the City Department of Public Works through the Green Halo Program, a waste disposal tracking system. The city has an authorized hauler, which is currently EDCO.*
- Stormwater Protection. *Any construction or operational activities that could potentially result in any water leaving the site as drainage shall be coordinated with the Lakewood Department of Public Works to assure proper procedures have been implemented (e.g., installation of stormwater wattles and track-out prevention devices.)*
- Temporary Banners. *(e.g., Now Open, Grand Opening, Sale, etc.) require a separate permit with a deposit and shall be removed before the allowed permit time expires.*
- Unauthorize Signs. *Roadside flag signs, A-frame/moveable signs, yard signs, pennants, balloons, inflatables, vehicles or trailers being used as portable signs, human sign twirlers, and mechanical mannequins are NOT ALLOWED. This does not include any onsite traffic control signs or devices.*

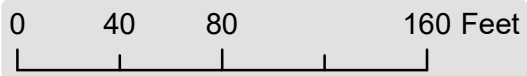
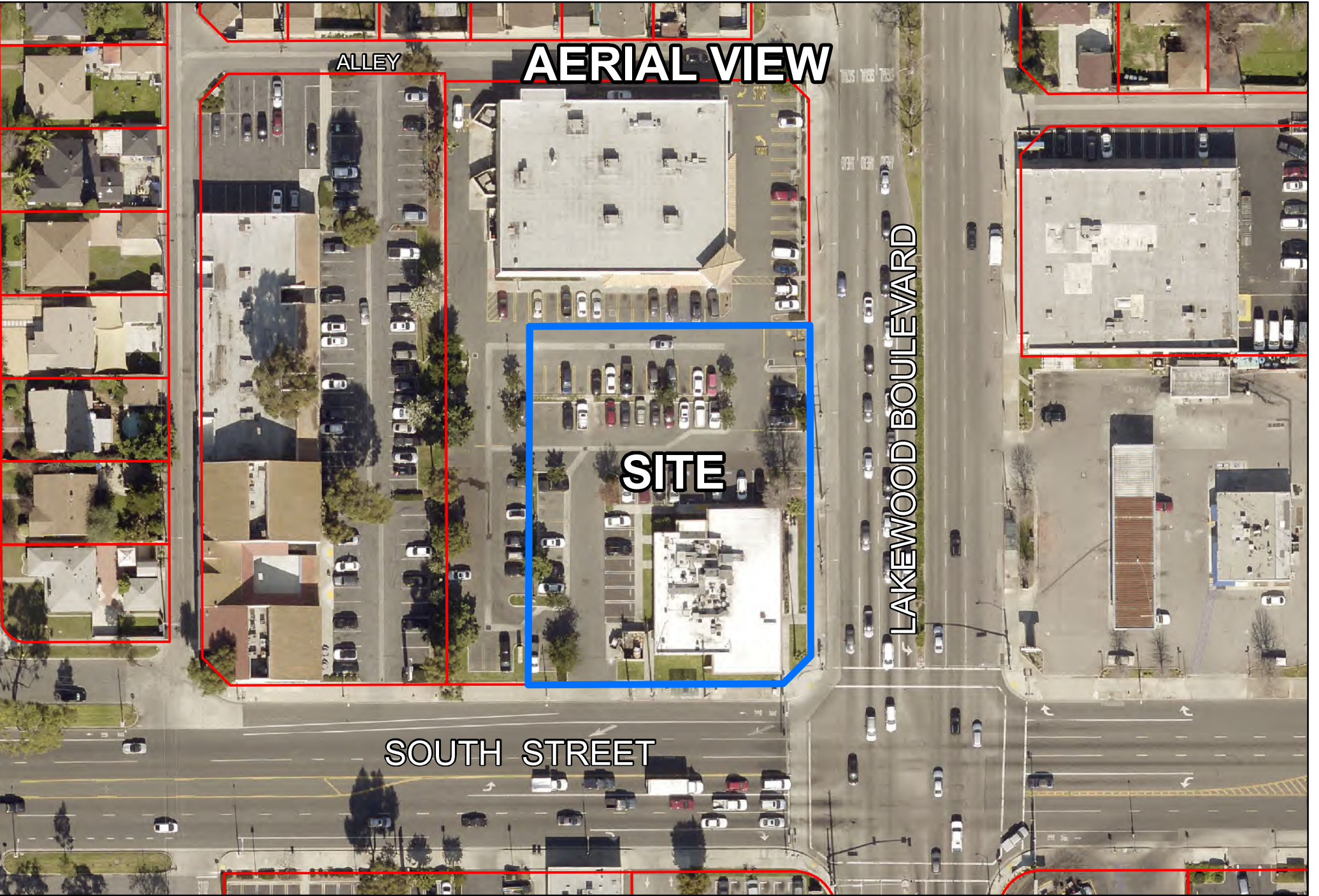
VICINITY MAP



Conditional Use Permit No. 1026
5809 Lakewood Boulevard
Dutch Bros Drive-Thru Restaurant

- R-1 (Single-Family Residential)
- M-F-R (Multi-Family Residential)
- C-1 (Neighborhood Commercial)
- C-3 (Intermediate Commercial)





CONDITIONAL USE PERMIT NO. 1026
589 LAKEWOOD BOULEVARD
DUTCH BROS DRIVE-THRU RESTAURANT



"The name DUTCH BROS., and all associated logos, distinctive designs, content, information, and other materials featured, displayed, contained herein, and made available by Dutch Bros., including but not limited to, the "look and feel" of the establishments and products, all text, images, colors, configurations, graphics, designs, illustrations, photographs, and pictures (collectively, the "Materials") are owned by and/or licensed by DB Franchising USA, LLC and are protected by copyright, trademark, trade dress, patent, and/or other intellectual property rights and unfair competition laws under the United States and foreign laws."

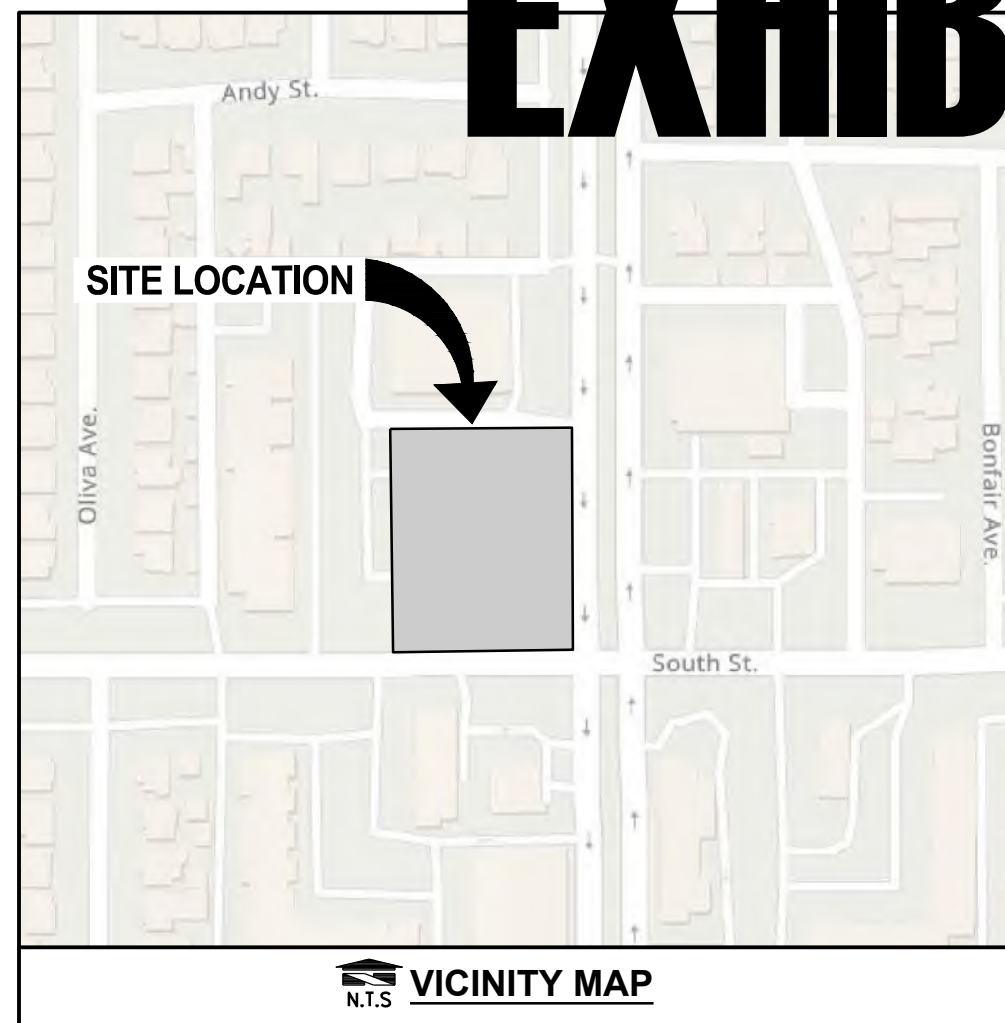
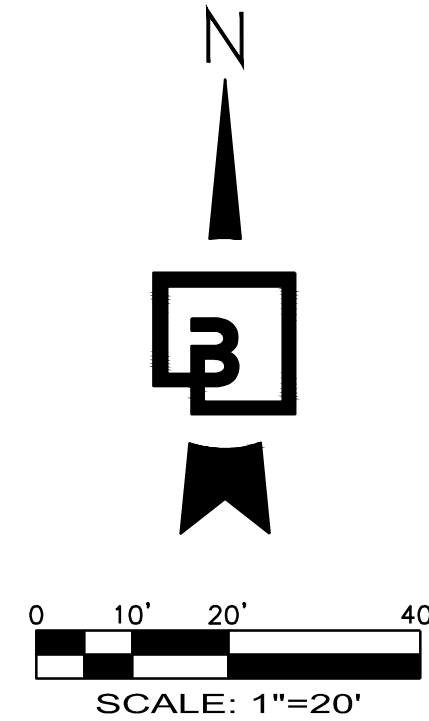


Know what's below.
Call before you dig.
Dial 811

DUTCH BROS COFFEE CA6405
5809 LAKEWOOD BLVD., LAKEWOOD, CA

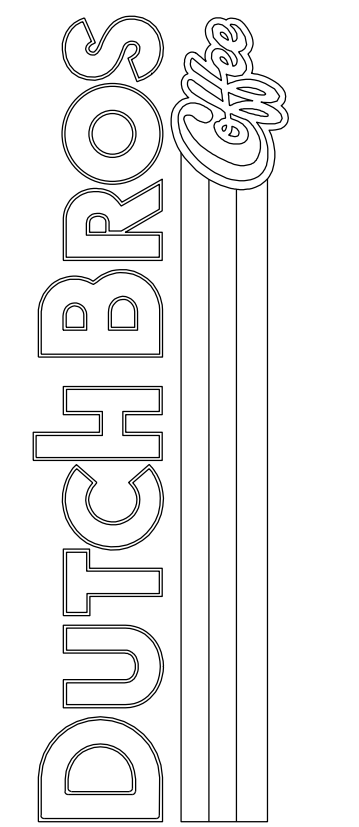
PRELIMINARY SITE PLAN

EXHIBIT "A"

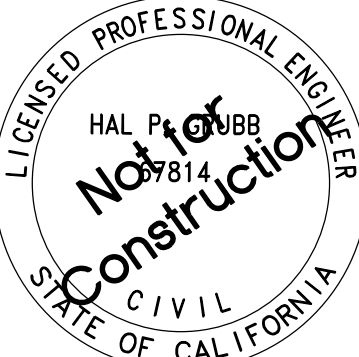


PRELIMINARY SITE PLAN
5809 LAKEWOOD BLVD.,
LAKEWOOD, CA

Title:

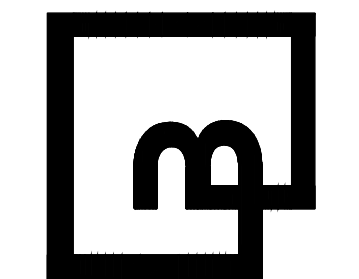


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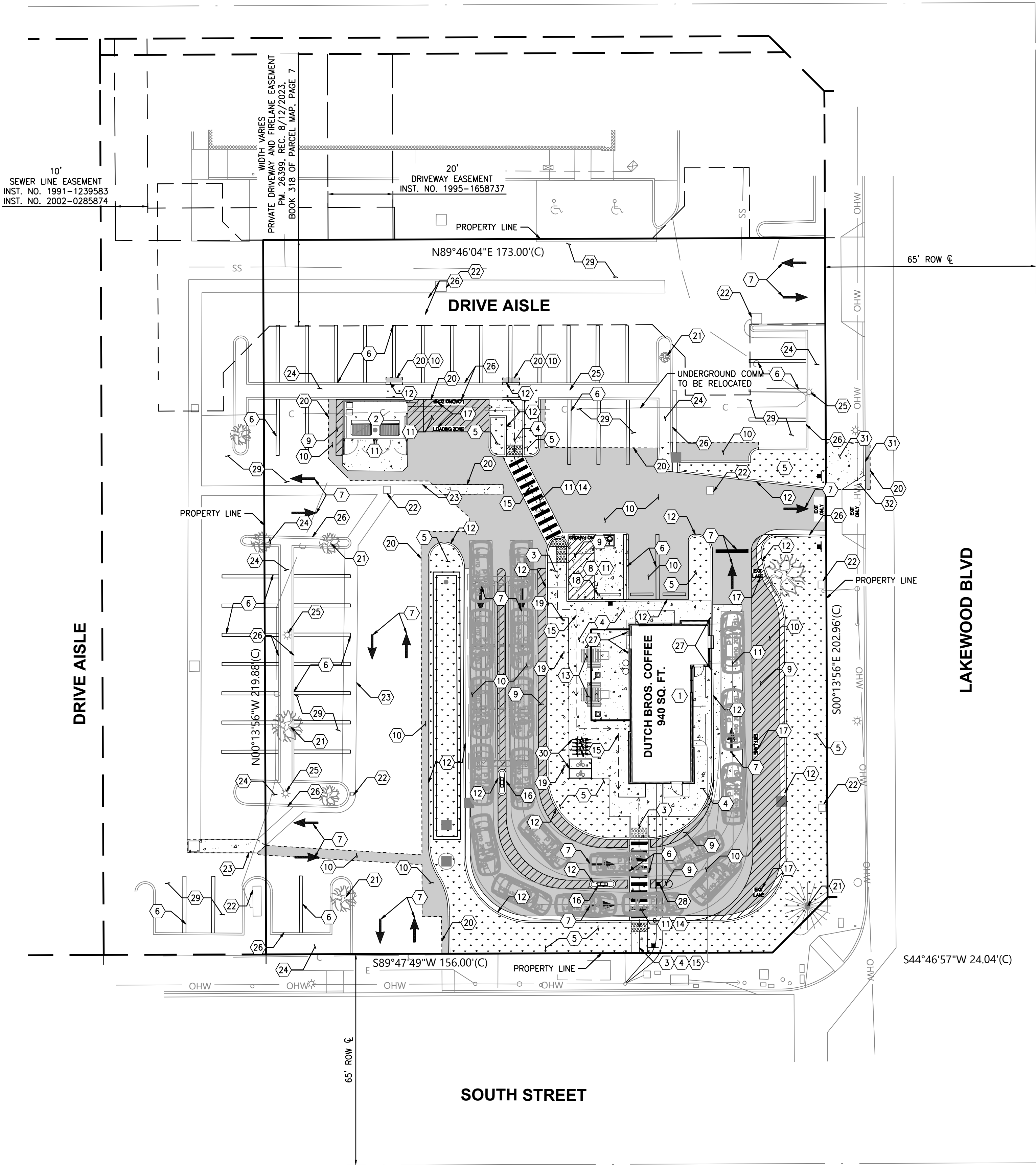


Scale:	Horizontal		Vertical
	1" = 20'		
Designed	PJB	PJB	Date
	Drawn	Checked	
Approved	MTL	MTL	4/10/25
	Approved	HPG	

Barghausen Consulting Engineers, Inc.
18215 72nd Avenue South
Kent, WA 98032
425.251.6222
barghausen.com



Job Number
23477
Sheet
C1.0
2019 DB
Final Design



CONSTRUCTION NOTES:

1. PROPOSED DUTCH BROS COFFEE STAND; REFER TO ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.
2. PROPOSED 20'X12' TRASH ENCLOSURE; REFER TO ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.
3. PROPOSED ADA RAMP.
4. PROPOSED PRIVATE CONCRETE SIDEWALK (WIDTH PER PLAN).
5. PROPOSED LANDSCAPING AREA; REFER TO LANDSCAPING PLANS FOR ADDITIONAL INFORMATION.
6. PROPOSED 4" WIDE WHITE REFLECTIVE PAINT, TYPICAL.
7. PROPOSED PAVEMENT TRAFFIC MARKINGS, TYPICAL.
8. PROPOSED VAN ACCESSIBLE PARKING SPACE AND AISLE WITH ALL REQUIRED SIGNAGE.
9. PROPOSED PAVEMENT STRIPING: 4" WIDE, 24" O.C., 45 DEGREE ANGLE, SAFETY WHITE.
10. PROPOSED ASPHALT PAVING.
11. PROPOSED CONCRETE PAVING.
12. PROPOSED CURB.
13. PROPOSED LOCATION OF FIXED 4 SEAT PICNIC TABLE.
14. PROPOSED CROSSWALK PAVEMENT STRIPING.
15. PROPOSED ACCESSIBLE PATH.
16. PROPOSED LOCATION SIGN.
17. PROPOSED 12" PAVEMENT LETTERING, TYPICAL.
18. PROPOSED PRECAST WHEEL STOP, TYPICAL.
19. PROPOSED CONCRETE ENCASED STEEL VEHICLE PROTECTION BOLLARD PLACED 5' ON CENTER, TYPICAL.
20. PROPOSED SAWCUT LINE.
21. EXISTING TREE, TYPICAL.
22. EXISTING UTILITY STRUCTURE AND ASSOCIATED UTILITIES SHOWN TO REMAIN.
23. EXISTING VALLEY GUTTER TO REMAIN.
24. EXISTING LANDSCAPING TO REMAIN.
25. EXISTING SITE LIGHT.
26. EXISTING CURB.
27. PROPOSED BOLLARDS.
28. PROPOSED "YIELD-ALTERNATIVE LANES" SIGN.
29. EXISTING ASPHALT PAVING TO BE SLURRY SEALED TO THE LIMITS SHOWN. RESTRIPE AS NEEDED, TYP.
30. PROPOSED SHORT-TERM AND LONG-TERM BICYCLE RACK.
31. PROPOSED SIDEWALK REPAIR PER CITY OF LAKEWOOD STANDARDS.
32. PROPOSED BARRIER CURB PER CITY OF LAKEWOOD.
33. PROPOSED DRIVEWAY REPAIR PER CITY OF LAKEWOOD STANDARDS.

PROJECT DATA

NAME: DUTCH BROS COFFEE - LAKEWOOD, CA

LOCATION: 5809 LAKEWOOD BLVD., LAKEWOOD, CA 90712

PROPOSED PARCEL AREA: 37,921 SF (0.87 AC)
PROPOSED AREA OF DISTURBANCE: 20,931 SF (0.48 AC)

BUILDING: 940 S.F.
TRASH ENCL: 12' X 20'

PARKING DATA:
STANDARD: 2 SPACES
ACCESSIBLE: 1 SPACE
EXISTING: 30 SPACES
TOTAL: 33 SPACES

TOTAL CAR STACKING: 19 VEHICLES

GROUND COVER:	BUILDING (TRASH ENCL. INCL.):	1,180 SF	(5.6%)
	PARKING AND MANEUVERING:		
	ASPHALT	9,644 SF	(46.1%)
	CONCRETE	2,809 SF	(13.4%)
	CONC. WALKWAYS:	2,191 SF	(10.5%)
	LANDSCAPE:	5,107 SF	(24.4%)
		20,931 SF	(100.0%)

Sheet List Table

Sheet Number	Sheet Title
C1.0	PRELIMINARY SITE PLAN
C2.0	GRADING AND DRAINAGE PLAN AND PROFILE
C3.0	BASIN MAP
LP-1	LANDSCAPE PLANTING PLAN
LP-2	LANDSCAPE PLANTING SPECIFICATIONS & DETAILS

LEGEND

BUILDING LINE	////
EXISTING CURB TO REMAIN	==
PROPOSED CURB	==
PROPOSED LANDSCAPING
PROPOSED ASPHALT	■
PROPOSED SLURRY SEAL	///
PROPOSED CONCRETE	■

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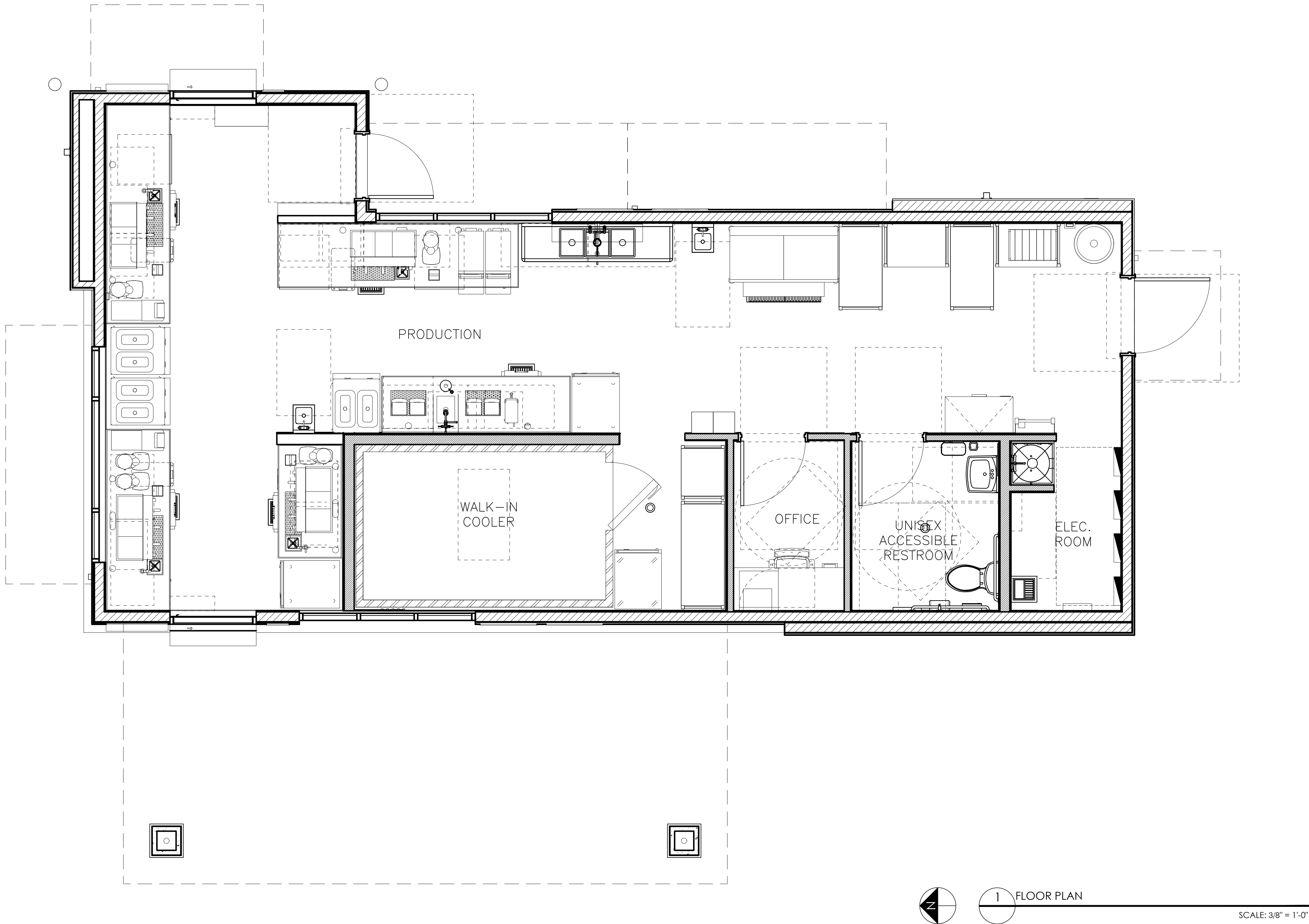


EXHIBIT “B”



ARMÉT DAVIS NEWLOVE &
ASSOCIATES, AIA ARCHITECTS

1330 OLYMPIC BLVD.
SANTA MONICA, CALIFORNIA 90404
PH 310 452-5533 FAX 310 450-4742

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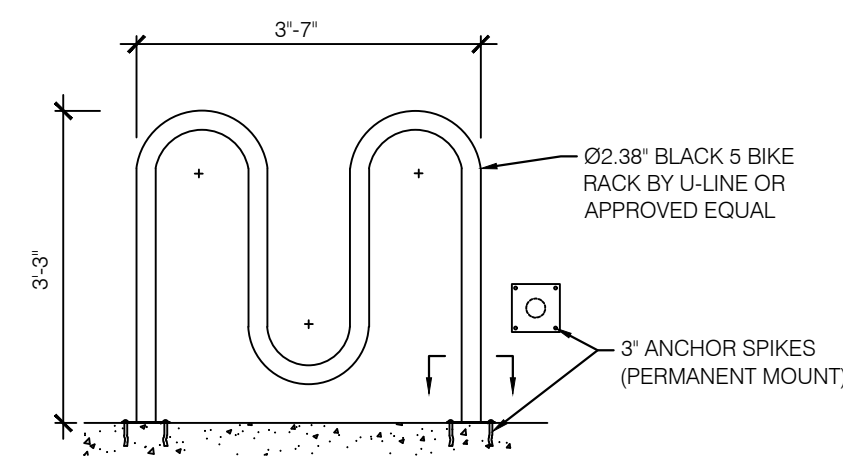
Project No: CA6405
Dutch Bros Coffee - New Freestanding Store
2550 - A1 PROTOTYPE
5809 LAKEWOOD BLVD.
LAKEWOOD, CA 90712

DATE: 06/17/2024
REV: DATE: DESCRIPTION:

SHEET NAME:
FLOOR PLAN/
DETAILS/ SCHEDULES

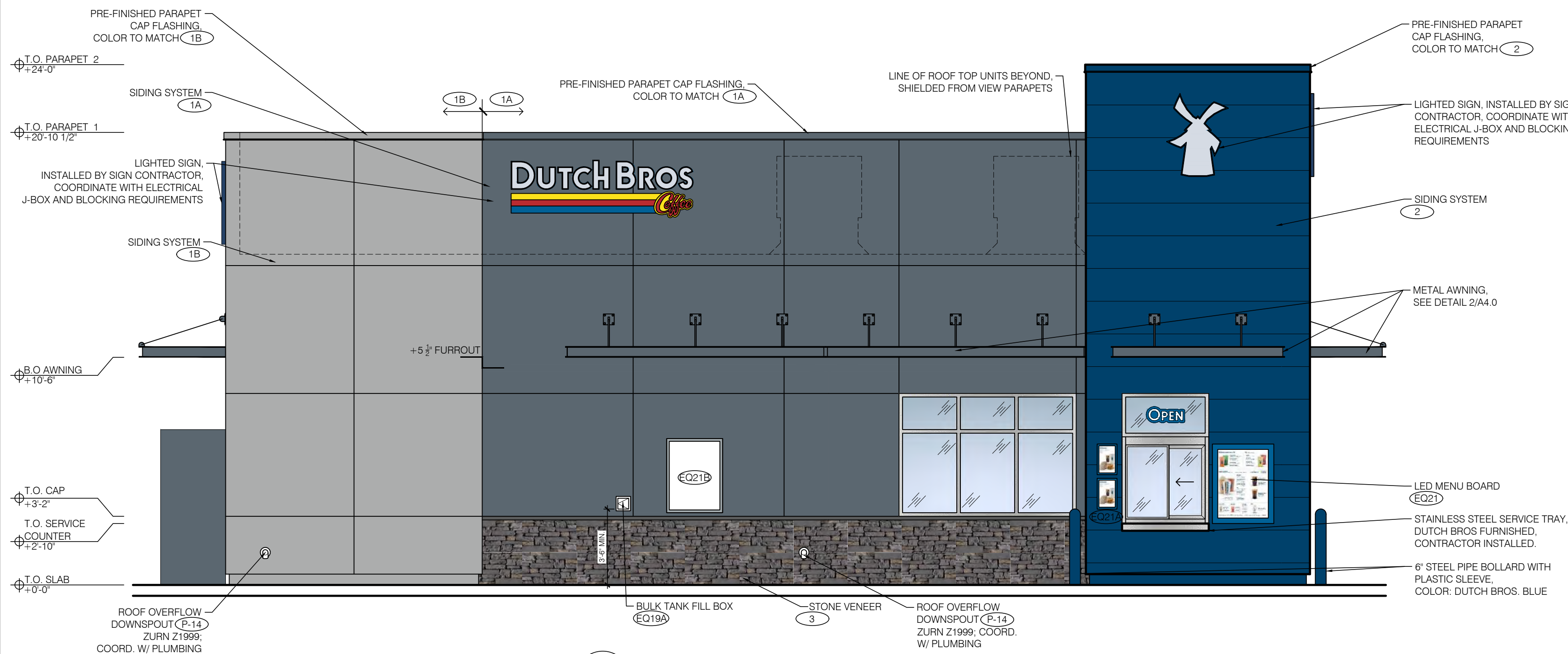
SHEET NUMBER:
A2.0

EXHIBIT "C"



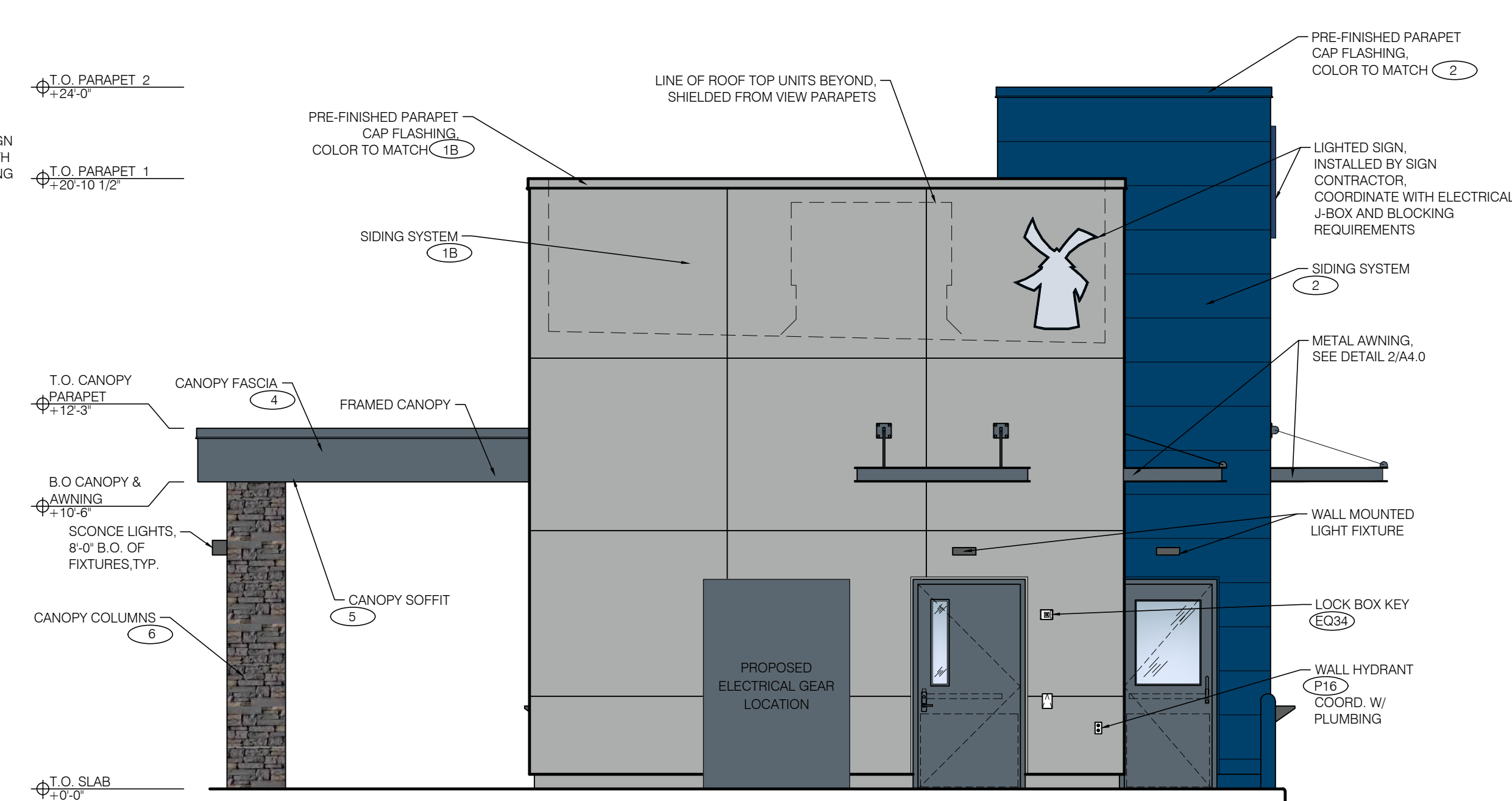
5 SHORT-TERM BICYCLE PARKING (5 SPACES)

SCALE: 1/2" = 1'-0"



4 ELEVATION - DRIVE-THRU WINDOW

SCALE: 1/4" = 1'-0"



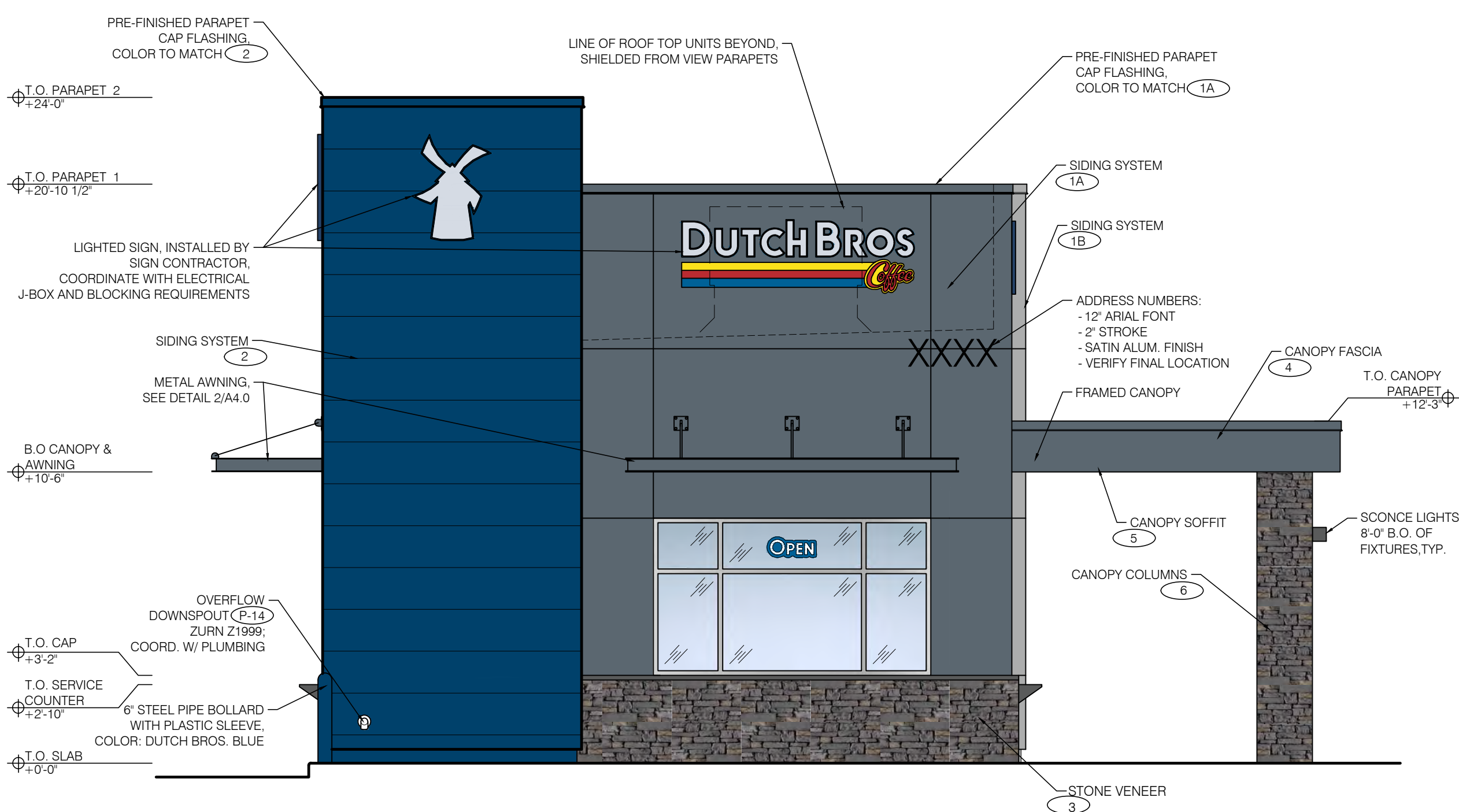
3 REAR ELEVATION

SCALE: 1/4" = 1'-0"



2 ELEVATION - WALK-UP WINDOW

SCALE: 1/4" = 1'-0"



1 FRONT ELEVATION

SCALE: 1/4" = 1'-0"

EXTERIOR FINISH SCHEDULE				
ID TAG	MATERIAL	MANUFACTURER	MODEL	REMARKS
ZONE 1 (BODY)				
1A	EXTERIOR CEMENT PLASTER	SHERWIN WILLIAMS	8656-11295 - BLDG DB DARK GRAY	3-COAT SYSTEM W/ ACRYLIC MEDIUM SAND FINISH; REVEALS AS SHOWN
1B	EXTERIOR CEMENT PLASTER	SHERWIN WILLIAMS	8656-12046 - BLDG DB LIGHT GRAY	3-COAT SYSTEM W/ ACRYLIC MEDIUM SAND FINISH; REVEALS AS SHOWN
ZONE 2 (TOWER) (Z2)				
2	FIBER CEMENT SIDING	NICHIHA	ILLUMINATION, AWP 1818 W/ OPEN OUTSIDE CORNERS	FACTORY PAINTED BLDG DB BLUE; CAP FLASHING TO MATCH BLDG DB BLUE
ZONE 3 (3'-2" BASE)				
3	STONE VENEER	ELDORADO STONE	CLIFFSTONE, BANFF SPRINGS	
	STONE VENEER SILL	ELDORADO STONE	SNAPPED EDGE WAINSCOT SILL; PEWTER	
ZONE 4 (FRAMED CANOPY)				
4	FASCIA	WESTERN STATES METAL ROOFING	T-GROOVE, 10"	3 SIDES; COLOR: BLDG DB DARK GRAY
5	SOFFIT	HEWN ELEMENTS	NATURAL NORTHWESTERN SPRUCE	1X6, T&G, $\frac{3}{8}$ " REEVAL
6	COLUMNS	ELDORADO STONE	CLIFFSTONE, BANFF SPRINGS	
NOTE: EXTERIOR DOORS TO BE PAINTED PER A6.1				



**ARMÉT DAVIS NEWLOVE &
ASSOCIATES, AIA ARCHITECTS**

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Project No: CA6405
Dutch Bros Coffee - New Freestanding Store
2550 - A1 PROTOTYPE
5809 LAKEWOOD BLVD.
LAKEWOOD, CA 90712

DATE: 06/17/2024		
REV:	DATE:	DESCRIPTION:

SHEET NAME:

BUILDING ELEVATIONS
COLOR

SHEET NUMBER:

A6.1

EXHIBIT “D”



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Project No: CA6405
Dutch Bros Coffee - New Freestanding Store
2550 - A1 PROTOTYPE

3809 LAKEWOOD BLVD.
LAKEWOOD, CA 90712

DATE: 06/17/2024

EV:	DATE:	DESCRIPTION:
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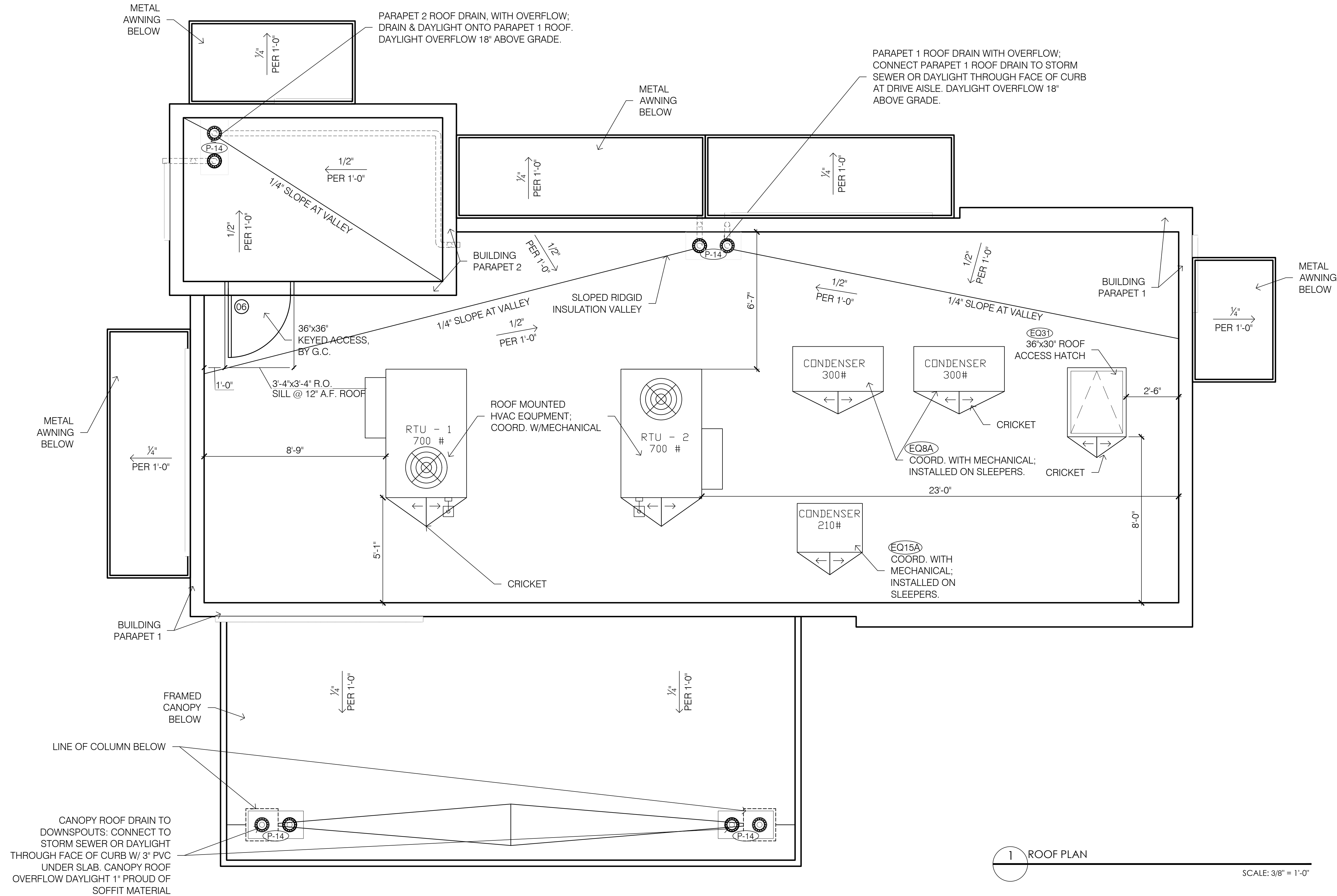
SHEET NAME:

ROOF PLAN/ ROOF DETAILS

SHEET NUMBER:

A4.0

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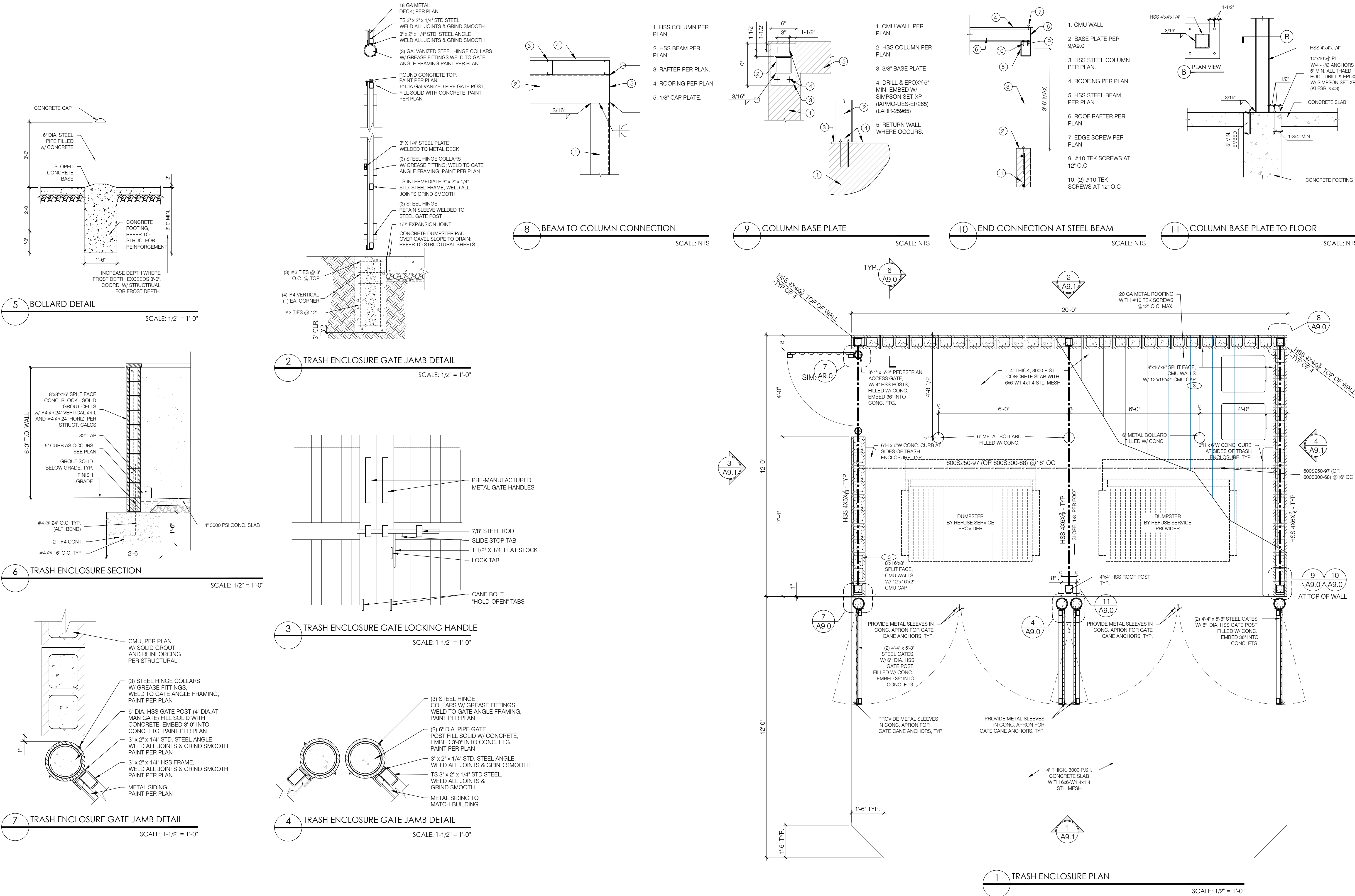


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EXHIBIT "E"

TRASH ENCLOSURE GENERAL NOTES:

1. REFER TO STRUCTURAL FOR MASONRY, CONCRETE, STRUCTURAL STEEL AND REINFORCING STEEL FOR SPECIFICATIONS.
2. PROVIDE LOCKING MECHANISM ON GATES AND PEDESTRIAN GATE.
3. PROVIDE SITE LIGHTING AT TRASH ENCLOSURE. MIN. 5 FOOT CANDLE.
4. GATE TO BE BUILT WITH 3"x 2" SQUARE STEEL TUBING WITH ALL JOINTS FULLY WELDED TOGETHER AND 1 CROSS MEMBER PER GATE. FRAME TO BE PRIMED AND PAINTED PER PLAN.



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Project No: CA6405
Dutch Bros Coffee - New Freestanding Store
2550 - A1 PROTOTYPE
5809 LAKEWOOD BLVD.
LAKEWOOD, CA 90712

DATE: 06/17/2024

REV: DATE: DESCRIPTION:

SHEET NAME:

TRASH ENCLOSURE
PLAN/ ELEVATIONS

SHEET NUMBER:

A9.0

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Y:\Active Projects\Dutch Bros\24 - CA6405 5809 Lakewood Blvd - Lakewood, CA\Drawings\CA6405 - A9.0 - TRASH ENCLOSURE - A1.dwg
11/12/2024 12:41:13 PM, Delle

EXHIBIT "F"

TRASH ENCLOSURE MATERIALS				
ID	MATERIAL	MANUFACTURER	COLOR	NOTES
3	SPLIT FACE 8X8X16	BASALITE	490	IF INTEGRAL COLOR BLOCK IS UNAVAILABLE, BLOCK TO BE PAINTED DB DARK GRAY
	CMU CAP	BASALITE	490	12X16X2
PT-2	PAIN	SHERWIN-WILLIAMS	8656-1125 - BLDG DB DARK GRAY	BLDG DB DARK GRAY, COLUMNS BODY, ROOF BEAMS & POSTS
PT-3	PAINT	SHERWIN-WILLIAMS	8656-12045 - BLDG DB BLUE	BLDG DB BLUE, GATE FRAMES & POSTS
PROVIDE ANTI GRAFFITI COATING AT EXTERIOR; INTERIOR SURFACE TO BE SMOOTH & NON-ABSORBENT				

TRASH ENCLOSURE GENERAL NOTES:

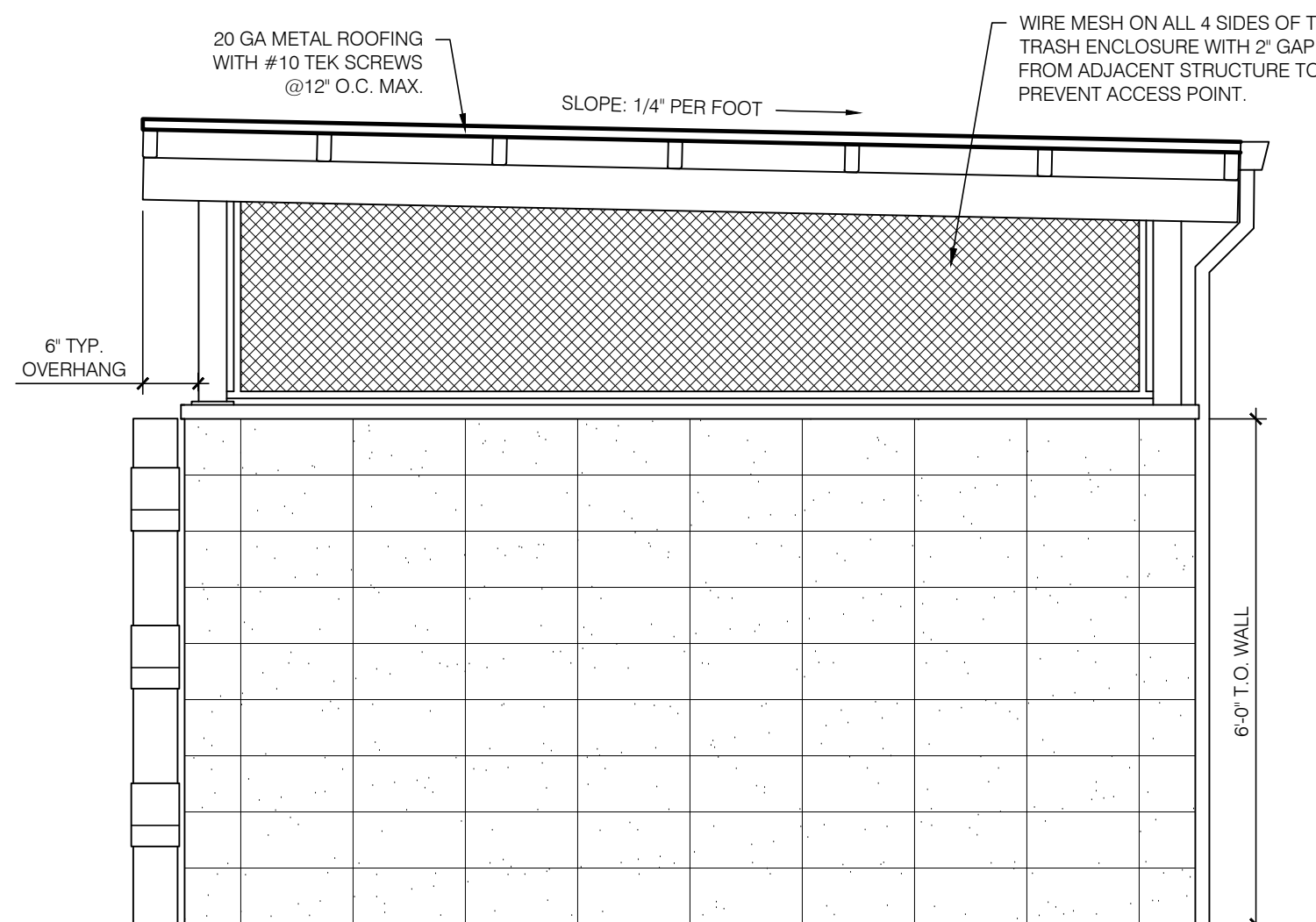
1. REFER TO STRUCTURAL FOR MASONRY, CONCRETE, STRUCTURAL STEEL AND REINFORCING STEEL FOR SPECIFICATIONS.
2. PROVIDE LOCKING MECHANISM ON GATES AND PEDESTRIAN GATE.
3. PROVIDE SITE LIGHTING AT TRASH ENCLOSURE. MIN. 5 FOOT CANDLE.
4. GATE TO BE BUILT WITH 3"X 2" SQUARE STEEL TUBING WITH ALL JOINTS FULLY WELDED TOGETHER AND 1 CROSS MEMBER PER GATE. FRAME TO BE PRIMED AND PAINTED PER PLAN.



ARMÉT DAVIS NEWLOVE &
ASSOCIATES, AIA ARCHITECTS

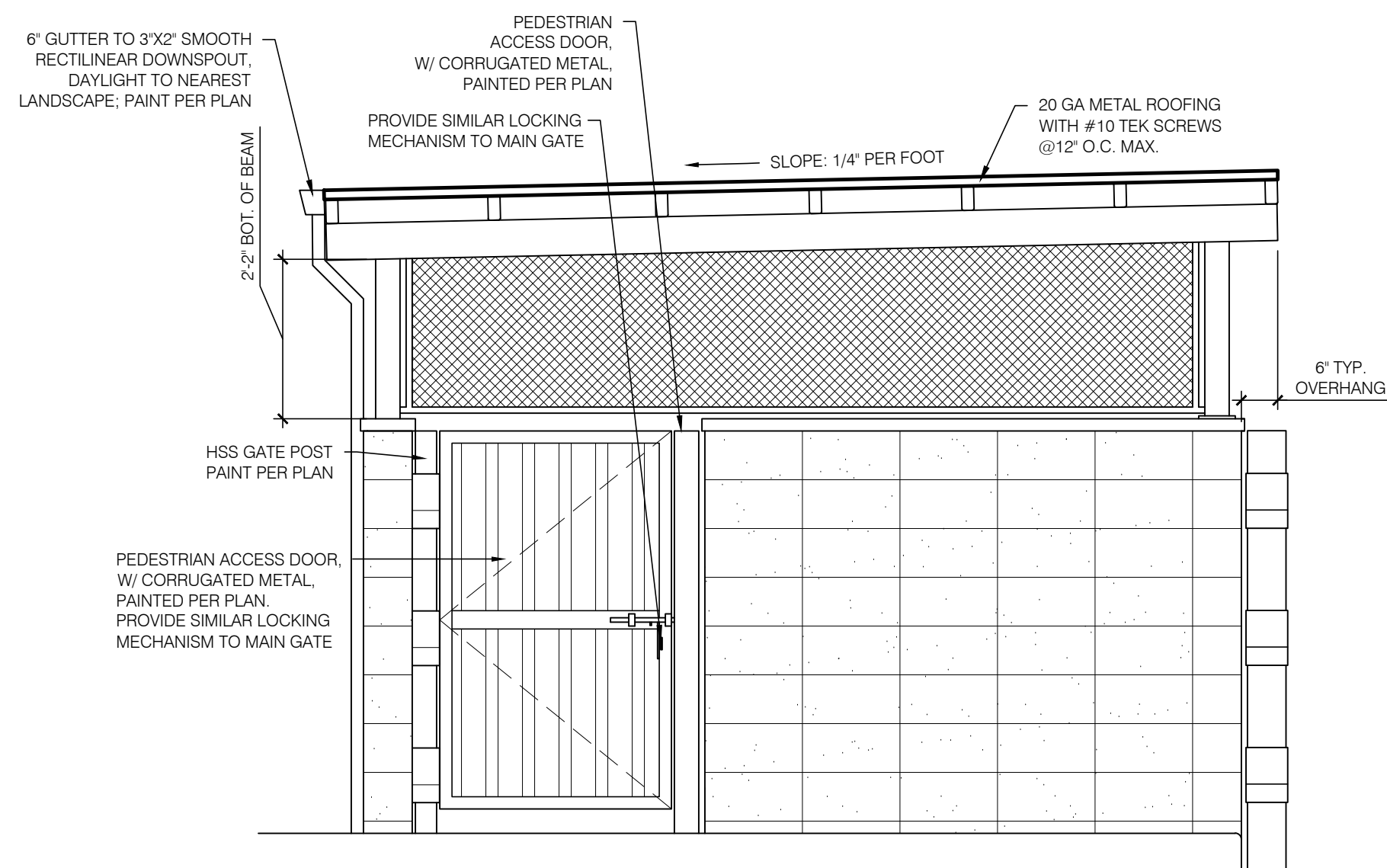
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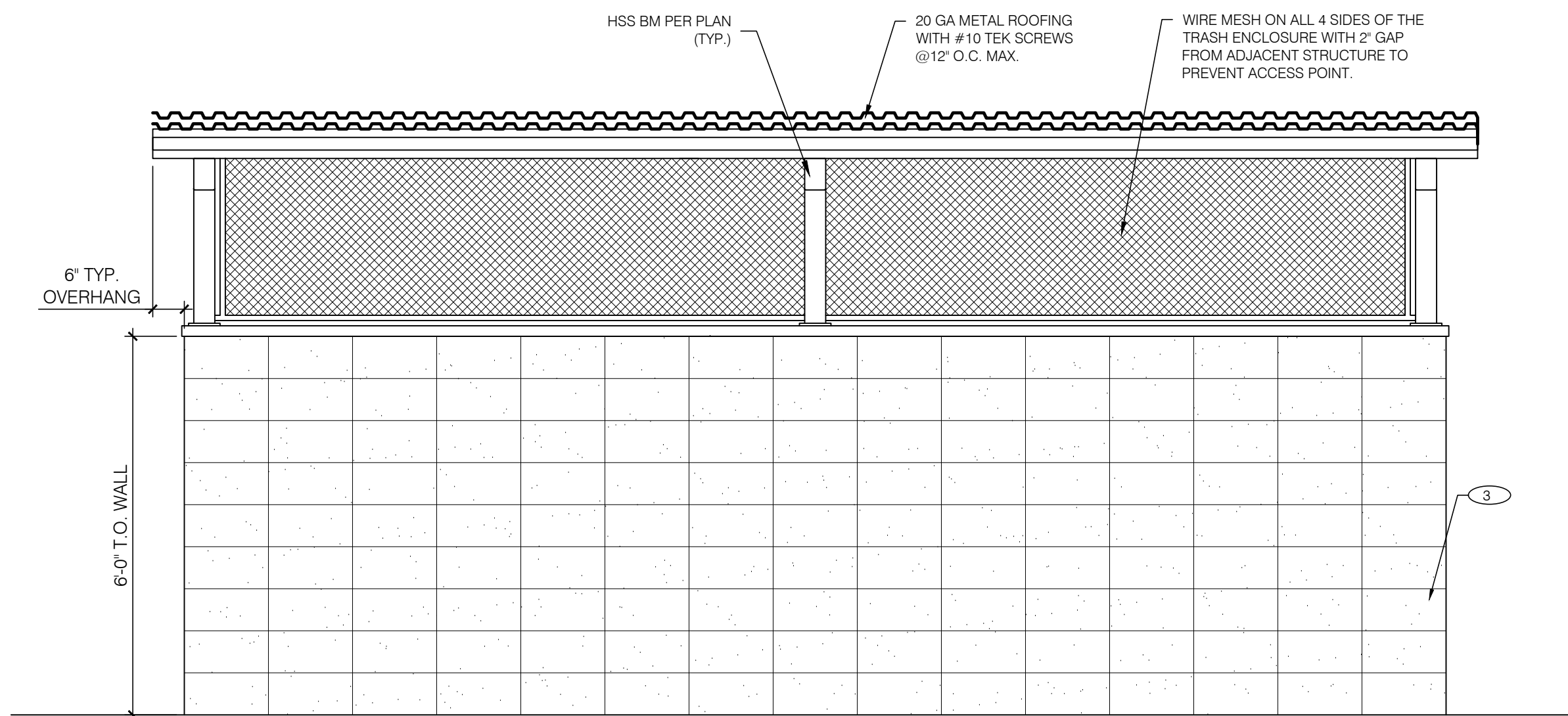
4 TRASH ENCLOSURE SIDE ELEVATION

SCALE: 1/2" = 1'-0"



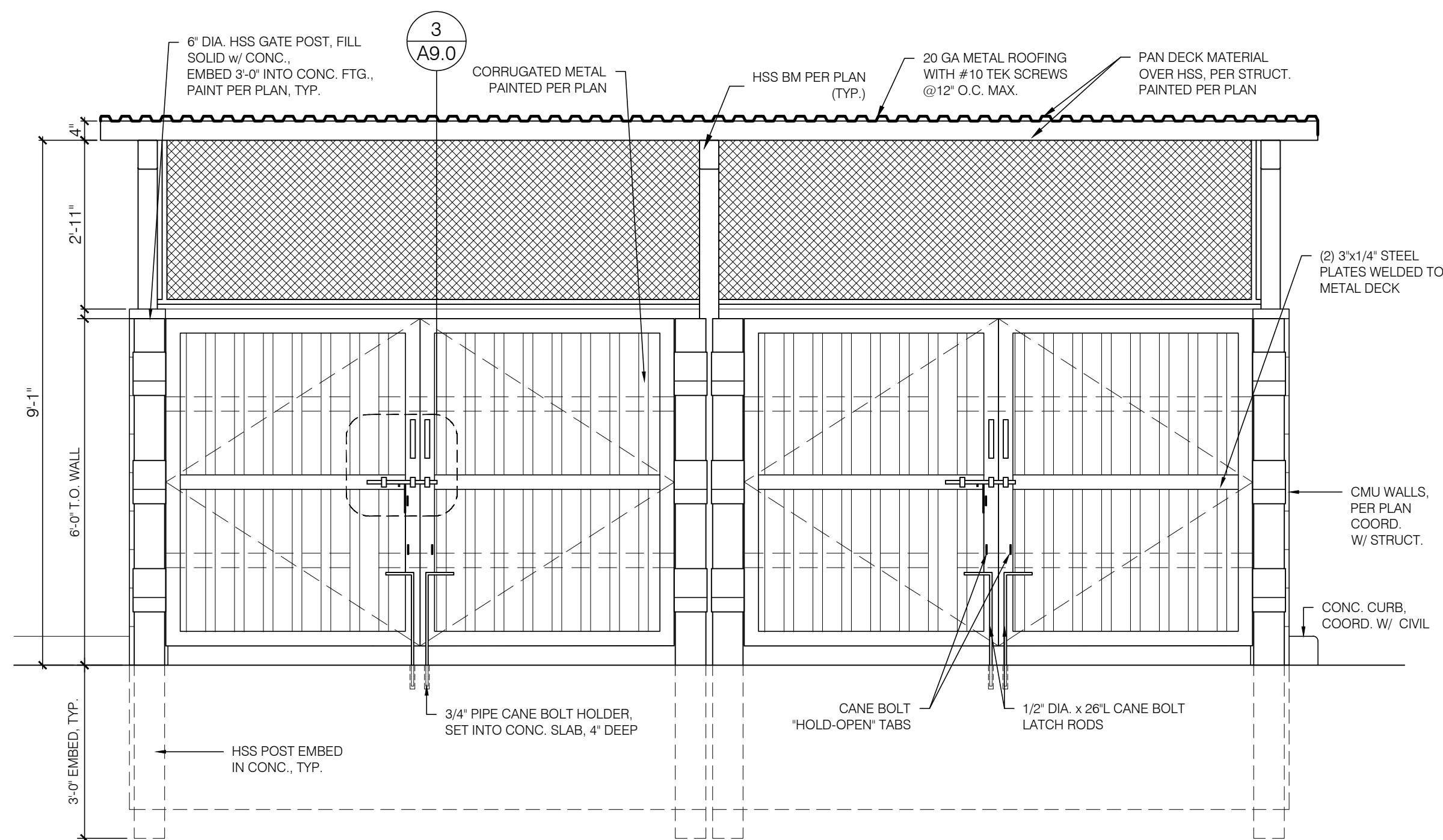
3 TRASH ENCLOSURE SIDE ELEVATION

SCALE: 1/2" = 1'-0"



2 TRASH ENCLOSURE REAR ELEVATION

SCALE: 1/2" = 1'-0"



1 TRASH ENCLOSURE FRONT ELEVATION

SCALE: 1/2" = 1'-0'

Project No: CA6405
Dutch Bros Coffee - New Freestanding Store
2550 - A1 PROTOTYPE

2550 - A1 PROTOTYPE
5809 LAKEWOOD BLVD.
LAKEWOOD, CA 90712

DATE: 06/17/2024

REV:	DATE:	DESCRIPTION:
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SHEET NAME

TRASH ENCLOSURE
PLAN/ ELEVATIONS

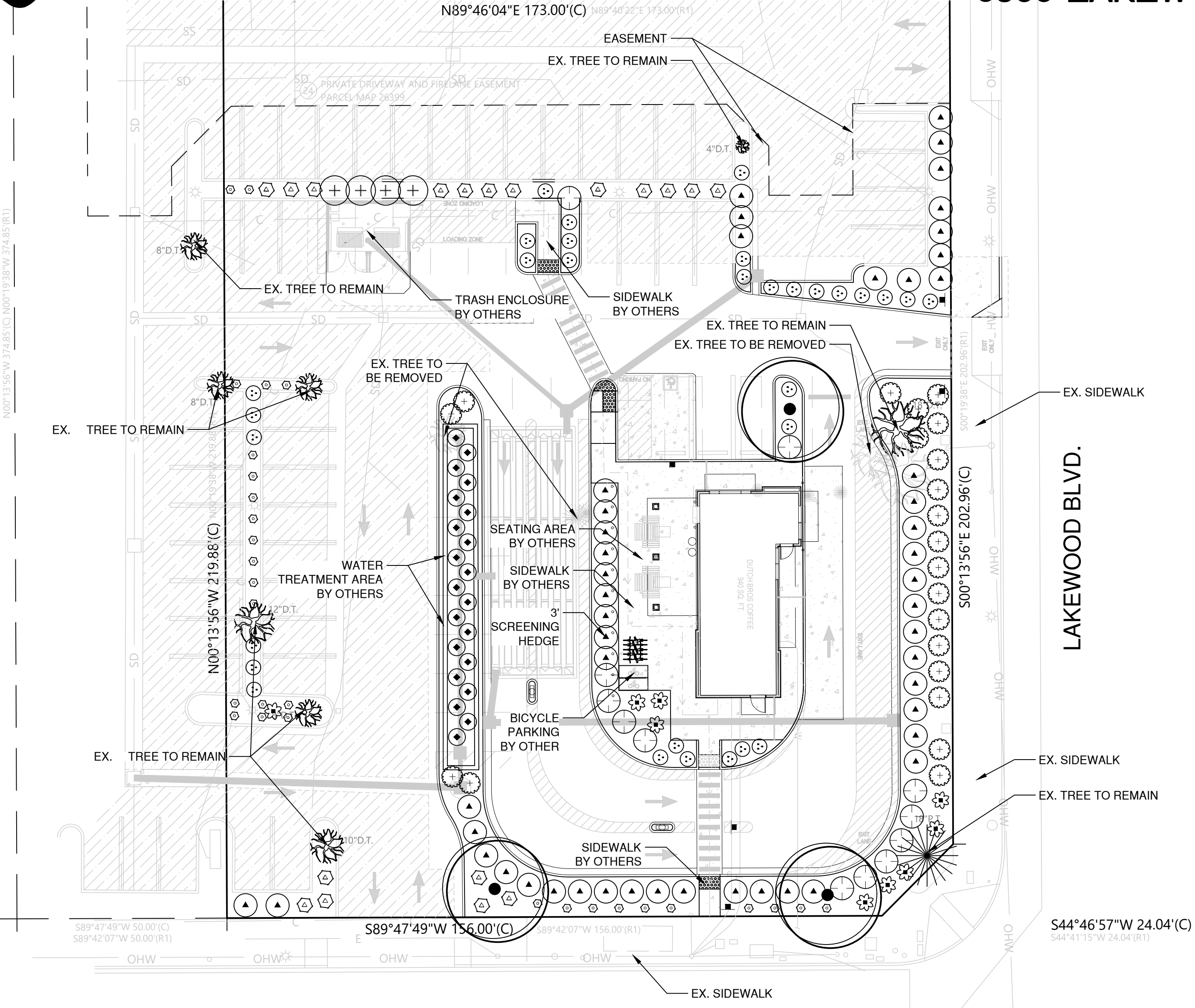
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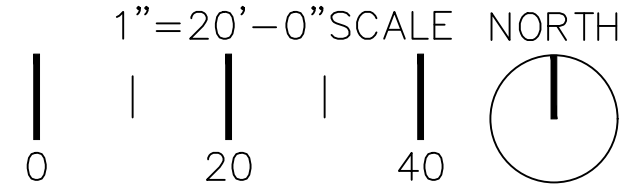


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SOUTH STREET

LAKEWOOD BLVD.



PROJECT TEAM

LANDSCAPE ARCHITECT:

EVERGREEN DESIGN GROUP
17875 VON KARMAN, STE 150
IRVINE, CA 92614
800-680-6630
WWW.EVERGREENDESIGNGROUP.COM
CONTACT: BLAKE TOMILLOSO-RHINEHART, PLA
EMAIL: BLAKE@EVERGREENDESIGNGROUP.COM

MULCHES


AFTER ALL PLANTING IS COMPLETE, CONTRACTOR SHALL INSTALL 3" THICK LAYER OF 1-1/2" SHREDDED WOOD MULCH, RECYCLED, NATURAL (UNDYED), OVER LANDSCAPE FABRIC IN ALL PLANTING AREAS (EXCEPT FOR TURF AND SEEDED AREAS). CONTRACTOR SHALL SUBMIT SAMPLES OF ALL MULCHES TO LANDSCAPE ARCHITECT AND OWNER FOR APPROVAL PRIOR TO CONSTRUCTION. ABSOLUTELY NO EXPOSED GROUND SHALL BE LEFT SHOWING ANYWHERE ON THE PROJECT AFTER MULCH HAS BEEN INSTALLED (SUBJECT TO THE CONDITIONS AND REQUIREMENTS OF THE "GENERAL GRADING AND PLANTING NOTES" AND SPECIFICATIONS).

ROOT BARRIERS

THE CONTRACTOR SHALL INSTALL ROOT BARRIERS NEAR ALL NEWLY-PLANTED TREES THAT ARE LOCATED WITHIN FIVE (5) FEET OF PAVING OR CURBS. ROOT BARRIERS SHALL BE "CENTURY" OR "DEEP-ROOT" 24" DEEP PANELS (OR EQUAL). BARRIERS SHALL BE LOCATED IMMEDIATELY ADJACENT TO HARDSCAPE. INSTALL PANELS PER MANUFACTURER'S RECOMMENDATIONS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR USE ROOT BARRIERS OF A TYPE THAT COMPLETELY ENCIRCLE THE ROOTBALL.

DECLARATION OF WATER EFFICIENT LANDSCAPE ORDINANCE

I HAVE COMPLIED WITH THE CRITERIA OF THE WATER EFFICIENT LANDSCAPE ORDINANCE AND APPLIED SUCH CRITERIA FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE DESIGN PLAN.


BLAKE TOMILLOSO-RHINEHART, LANDSCAPE ARCHITECT
DATE 3.5.2025
CA LICENSE #6255
8-31-26
REGISTRATION
EXP. DATE

MWEO NOTES


- A DIAGRAM OF THE IRRIGATION PLAN SHOWING HYDROZONES SHALL BE KEPT WITH THE IRRIGATION CONTROLLER FOR SUBSEQUENT MANAGEMENT PURPOSES.
- A CERTIFICATE OF COMPLETION SHALL BE FILLED OUT AND CERTIFIED BY EITHER THE DESIGNER OF THE LANDSCAPE PLANS, IRRIGATION PLANS, OR THE LICENSED LANDSCAPE CONTRACTOR FOR THE PROJECT.
- AN IRRIGATION AUDIT REPORT BY A DISINTERESTED THIRD PARTY SHALL BE COMPLETED AT THE TIME OF FINAL INSPECTION
- THE CONTRACTOR SHALL PREPARE A SOILS MANAGEMENT REPORT. THE SOIL MANAGEMENT REPORT SHALL BE BOTH INTEGRATED INTO THE PLANS AND SUBMITTED AS A SEPARATE DOCUMENT.
- PRESSURE REGULATING DEVICES ARE REQUIRED IF WATER PRESSURE IS BELOW OR EXCEEDS THE RECOMMENDED PRESSURE OF THE SPECIFIED IRRIGATION DEVICES.
- CHECK VALVES OR ANTI-DRAIN VALVES ARE REQUIRED ON ALL SPRINKLER HEADS WHERE LOW POINT DRAINAGE COULD OCCUR.

SOILS NOTE

FOR SOILS LESS THAN 6% ORGANIC MATTER IN THE TOP 6 INCHES OF SOIL, COMPOST AT A RATE OF A MINIMUM OF FOUR CUBIC YARDS PER 1,000 SQUARE FEET OF PERMEABLE AREA SHALL BE INCORPORATED TO A DEPTH OF SIX INCHES INTO THE SOIL EXCEPT WITHIN THE TPZ OF PROTECTED TREES, WHICH SHALL RECEIVE 4 - 6" OF HARDWOOD CHIP MULCH.

DUTCH BROS COFFEE CA6405
5809 LAKEWOOD BLVD., LAKEWOOD, CA

PLANT SCHEDULE

SYMBOL	BOTANICAL / COMMON NAME	SIZE	WUCOLS	CAL	HEIGHT	QTY
TREES						
	GEIJERA PARVIFLORA AUSTRALIAN WILLOW	15 GAL	L	1.5" CAL.	6'-8'	3

SYMBOL	BOTANICAL / COMMON NAME	SIZE	WUCOLS	MATURE SIZE (W'X'H')	ADDITIONAL	SPACING	QTY
SHRUBS							
	AGAVE ATTENUATA FOXTAIL AGAVE	5 GAL	L			48" o.c.	9
	ARCTOSTAPHYLOS EDMUNDII 'CARMEL SUR' CARMEL SUR LITTLE SUR MANZANITA	1 GAL	L			60" o.c.	21
	CHONDROPETALUM TECTORUM SMALL CAPE RUSH	5 GAL	L			48" o.c.	21
	DIANELLA REVOLUTA 'VARIEGATED' VARIEGATED FLAX LILY	1 GAL	L			24" o.c.	29
	HETEROMELES ARBUTIFOLIA 'DAVIS GOLD' DAVIS GOLD TOYON	5 GAL	L			84" o.c.	4
	MUHLBERGIA CAPILLARIS PINK MUHLY GRASS	1 GAL	L			42" o.c.	18
	MUHLBERGIA DUBIA PINE MUHLY	1 GAL	L			42" o.c.	31
	MUHLBERGIA RIGENS DEER GRASS	5 GAL	L			66" o.c.	12
	OLEA EUROPAEA 'MONTRA' LITTLE OLLIE® OLIVE	5 GAL	L			66" o.c.	50

LANDSCAPE CALCULATIONS

GENERAL CALCULATIONS:	
LOT AREA:	37,962 SF
LANDSCAPE AREA REQUIRED:	1,518 SF (4%)
LANDSCAPE AREA PROVIDED:	5,107 SF (13.5%)
TURF AREAS PROPOSED:	0 SF (0%)
TREES REQUIRED:	5 TREES (1 PER 300SF)
TREES PROVIDED:	2 PROPOSED + 3 EXISTING

GENERAL GRADING AND PLANTING NOTES

- BY SUBMITTING A PROPOSAL FOR THE LANDSCAPE PLANTING SCOPE OF WORK, THE CONTRACTOR CONFIRMS THAT HE HAS READ, AND WILL COMPLY WITH, THE ASSOCIATED NOTES, SPECIFICATIONS, AND DETAILS WITH THIS PROJECT.
- THE GENERAL CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL EXISTING VEGETATION (EXCEPT WHERE NOTED TO REMAIN).
- IN THE CONTEXT OF THESE PLANS, NOTES, AND SPECIFICATIONS, "FINISH GRADE" REFERS TO THE FINAL ELEVATION OF THE SOIL SURFACE (NOT TOP OF MULCH) AS INDICATED ON THE GRADING PLANS.
 - BEFORE STARTING WORK, THE LANDSCAPE CONTRACTOR SHALL VERIFY THAT THE ROUGH GRADES OF ALL LANDSCAPE AREAS ARE WITHIN +/-0.1' OF FINISH GRADE. SEE SPECIFICATIONS FOR MORE DETAILED INSTRUCTION ON TURF AREA AND PLANTING BED PREPARATION.
 - CONSTRUCT AND MAINTAIN FINISH GRADES AS SHOWN ON GRADING PLANS, AND CONSTRUCT AND MAINTAIN SLOPES AS RECOMMENDED BY THE GEOTECHNICAL REPORT. ALL LANDSCAPE AREAS SHALL HAVE POSITIVE DRAINAGE AWAY FROM STRUCTURES AT THE MINIMUM SLOPE SPECIFIED IN THE REPORT AND ON THE GRADING PLANS, AND AREAS OF POTENTIAL PONDING SHALL BE REGRADED TO BLEND IN WITH THE SURROUNDING GRADES AND ELIMINATE PONDING POTENTIAL.
 - THE LANDSCAPE CONTRACTOR SHALL DETERMINE WHETHER OR NOT THE EXPORT OF ANY SOIL WILL BE NEEDED, TAKING INTO ACCOUNT THE ROUGH GRADE PROVIDED, THE AMOUNT OF SOIL AMENDMENTS TO BE ADDED (BASED ON A SOIL TEST, PER SPECIFICATIONS), AND THE FINISH GRADES TO BE ESTABLISHED.
 - ENSURE THAT THE FINISH GRADE IN SHRUB AREAS IMMEDIATELY ADJACENT TO WALKS AND OTHER WALKING SURFACES, AFTER INSTALLING SOIL AMENDMENTS, IS 3" BELOW THE ADJACENT FINISH SURFACE, IN ORDER TO ALLOW FOR PROPER MULCH DEPTH. TAPER THE SOIL SURFACE TO MEET FINISH GRADE, AS SPECIFIED ON THE GRADING PLANS, AT APPROXIMATELY 18" AWAY FROM THE WALKS.
 - ENSURE THAT THE FINISH GRADE IN TURF AREAS IMMEDIATELY ADJACENT TO WALKS AND OTHER WALKING SURFACES, AFTER INSTALLING SOIL AMENDMENTS, IS 1" BELOW THE FINISH SURFACE OF THE WALKS. TAPER THE SOIL SURFACE TO MEET FINISH GRADE, AS SPECIFIED ON THE GRADING PLANS, AT APPROXIMATELY 18" AWAY FROM THE WALKS.
 - SHOULD ANY CONFLICTS AND/OR DISCREPANCIES ARISE BETWEEN THE GRADING PLANS, GEOTECHNICAL REPORT, THESE NOTES AND PLANS, AND ACTUAL CONDITIONS, THE CONTRACTOR SHALL IMMEDIATELY BRING SUCH ITEMS TO THE ATTENTION OF THE LANDSCAPE ARCHITECT, GENERAL CONTRACTOR, AND OWNER.
- ALL PLANT LOCATIONS ARE DIAGRAMMATIC. ACTUAL LOCATIONS SHALL BE VERIFIED WITH THE LANDSCAPE ARCHITECT OR DESIGNER PRIOR TO PLANTING. THE LANDSCAPE CONTRACTOR SHALL ENSURE THAT ALL REQUIREMENTS OF THE PERMITTING AUTHORITY ARE MET (I.E., MINIMUM PLANT QUANTITIES, PLANTING METHODS, TREE PROTECTION METHODS, ETC.).
 - THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR DETERMINING PLANT QUANTITIES; PLANT QUANTITIES SHOWN ON LEGENDS AND CALLOUTS ARE FOR GENERAL INFORMATION ONLY. IN THE EVENT OF A DISCREPANCY BETWEEN THE PLAN AND THE PLANT LEGEND, THE PLANT QUANTITY AS SHOWN ON THE PLAN (FOR INDIVIDUAL SYMBOLS) OR CALLOUT (FOR GROUNDCOVER PATTERNS) SHALL TAKE PRECEDENCE.
 - NO SUBSTITUTIONS OF PLANT MATERIALS SHALL BE ALLOWED WITHOUT THE WRITTEN PERMISSION OF THE LANDSCAPE ARCHITECT. IF SOME OF THE PLANTS ARE NOT AVAILABLE, THE LANDSCAPE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT IN WRITING (VIA PROPER CHANNELS).
 - THE CONTRACTOR SHALL, AT A MINIMUM, PROVIDE REPRESENTATIVE PHOTOS OF ALL PLANTS PROPOSED FOR THE PROJECT. THE CONTRACTOR SHALL ALLOW THE LANDSCAPE ARCHITECT AND THE OWNER/OWNER'S REPRESENTATIVE TO INSPECT, AND APPROVE OR REJECT, ALL PLANTS DELIVERED TO THE JOBSITE. REFER TO SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS FOR SUBMITTALS.
- THE CONTRACTOR SHALL MAINTAIN THE LANDSCAPE IN A HEALTHY CONDITION FOR 90 DAYS AFTER ACCEPTANCE BY THE OWNER. REFER TO SPECIFICATIONS FOR CONDITIONS OF ACCEPTANCE FOR THE START OF THE MAINTENANCE PERIOD, AND FOR FINAL ACCEPTANCE AT THE END OF THE MAINTENANCE PERIOD.
- SEE SPECIFICATIONS AND DETAILS FOR FURTHER REQUIREMENTS.

IRRIGATION CONCEPT

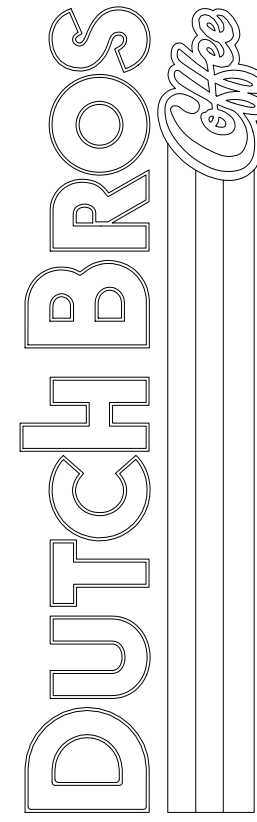
- AN AUTOMATIC IRRIGATION SYSTEM SHALL BE INSTALLED AND OPERATIONAL BY THE TIME OF FINAL INSPECTION. THE ENTIRE IRRIGATION SYSTEM SHALL BE INSTALLED BY A LICENSED AND QUALIFIED IRRIGATION CONTRACTOR.
- THE IRRIGATION SYSTEM WILL OPERATE ON POTABLE WATER, AND THE SYSTEM WILL HAVE APPROPRIATE BACKFLOW PREVENTION DEVICES INSTALLED TO PREVENT CONTAMINATION OF THE POTABLE SOURCE.
- ALL NON-TURF PLANTED AREAS SHALL BE DRIP IRRIGATED. SODDED AND SEEDED AREAS SHALL BE IRRIGATED WITH SPRAY OR ROTOR HEADS AT 100% HEAD-TO-HEAD COVERAGE.
- ALL PLANTS SHARING SIMILAR HYDROZONE CHARACTERISTICS SHALL BE PLACED ON A VALVE DEDICATED TO PROVIDE THE NECESSARY WATER REQUIREMENTS SPECIFIC TO THAT HYDROZONE.
- THE IRRIGATION SYSTEM SHALL BE DESIGNED AND INSTALLED, TO THE MAXIMUM EXTENT POSSIBLE, TO CONSERVE WATER BY USING THE FOLLOWING DEVICES AND SYSTEMS: MATCHED PRECIPITATION RATE TECHNOLOGY ON ROTOR AND SPRAY HEADS (WHEREVER POSSIBLE), RAIN SENSORS, AND MULTI-PROGRAM COMPUTERIZED IRRIGATION CONTROLLERS FEATURING SENSORY INPUT CAPABILITIES.

PRELIMINARY NOT FOR CONSTRUCTION

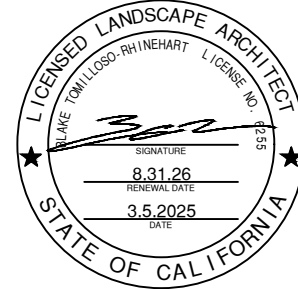
EXHIBIT "G"

Title: LANDSCAPE PLANTING PLAN

5809 LAKEWOOD BLVD.,
LAKEWOOD, CA



For:



Scale:

Horizontal

Vertical

Designed BTR

Drawn BTR

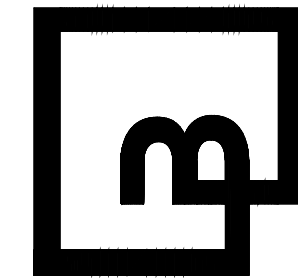
Checked BTR

Approved BTR

Date: 5.2025

Barghausen
Consulting Engineers, Inc.

18215 72nd Avenue South
Kent, WA 98032
425.251.6222
barghausen.com



Job Number

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RESOLUTION NO. -2025

A RESOLUTION OF THE PLANNING AND ENVIRONMENT COMMISSION OF THE CITY OF LAKEWOOD APPROVING THE APPLICATION FOR CONDITIONAL USE PERMIT NO. 1026, 5809 LAKEWOOD BOULEVARD, LAKEWOOD, CALIFORNIA, FOR A DRIVE-THRU RESTAURANT.

THE PLANNING AND ENVIRONMENT COMMISSION OF THE CITY OF LAKEWOOD DOES HEREBY FIND, RESOLVE AND DETERMINE AS FOLLOWS:

SECTION 1. The Planning and Environment Commission of the City of Lakewood, having had submitted to it the application of Brianna Uy, Barghausen, representing Dutch Bros lessee of Qing kai Investment, LLC, owner of an interest in the following described real property, requesting a Conditional Use Permit for a drive-thru restaurant, pursuant to the provisions under Sections 9347.D.6 of the Lakewood Municipal Code on that certain real property within the City of Lakewood described as a portion of Lot 2 of Tract 26399 in Map Book 318, pages 7-9, in the City of Lakewood, County of Los Angeles, State of California as shown on maps recorded in the office of the County Recorder of said County and more particularly described as 5809 Lakewood Boulevard, Lakewood, California; all as shown in the attached minutes and report of the Planning and Environment Commission, attached hereto and made a part hereof as though set forth in full, the Planning and Environment Commission does hereby find and determine as provided in this Resolution.

SECTION 2. The Planning and Environment Commission finds that the proposed conditional use is Categorically Exempt from the California Environmental Quality Act (CEQA) based on Sections 15301.(1).(3) and 15303.(c) of the CEQA Guidelines, as amended.

SECTION 3. The Planning and Environment Commission of the City of Lakewood does hereby report that a public hearing was held before the Planning and Environment Commission in respect to said application on the 1st day of May, 2025, and the Planning and Environment Commission does hereby find and determine that said application, subject to the conditions hereinafter specified, should be granted for the following reasons:

A. The request is for approval of Conditional Use Permit No. 1026 for a drive-thru restaurant at 5809 Lakewood Boulevard, Lakewood, all as shown on Exhibits "A," "B," "C," "D," "E," "F," and "G."

B. The proposed conditional use is found not to be in conflict with the goals of the General Plan, nor is the proposed use in conflict with the Commercial land use designation of the General Plan.

C. The nature, condition, and development of adjacent uses, buildings, and structures have been considered, and it has been found that the proposed drive-thru restaurant does not adversely affect nor is it materially detrimental to adjacent uses, buildings, or structures provided that the conditions contained herein are met and maintained at all times.

D. A traffic study and queuing study were considered for this project and Lakewood Boulevard and South Street are found to be adequate to serve the traffic generated by the site. Thus, no adverse effect is anticipated on existing roads and circulation as a consequence of this application.

E. Notification of a public hearing has been made, pursuant to Section 9422, et seq., of the Lakewood Municipal Code and State law.

SECTION 4. The Planning and Environment Commission of the City of Lakewood, based upon the aforementioned findings and determinations, hereby grants the use as requested in Conditional Use Permit No. 1026 provided the use is operated in compliance with the following conditions at all times:

A. The conditions, unless otherwise specified herein, shall be completed within three years from the date of the granting of this application, and not thereafter violated or deviated from except where authorized by amendment to this Resolution adopted in accordance with the provisions of this Resolution and the Lakewood Municipal Code. The granting of the Conditional Use Permit and this Resolution, and any modification or change thereof, shall not be effective for any purpose until a certified copy of this Resolution (Exhibits excluded) has been recorded in the Office of the Los Angeles County Recorder. The granting of this Conditional Use Permit, subject to the conditions herein set forth, are binding on their heirs, assigns, and successors in interest of the applicant and their heirs, assigns, and successors in interest.

B. Approval for the drive-thru restaurant, located at 5809 Lakewood Boulevard, Lakewood, shall be based on Exhibits “A,” “B,” “C,” “D,” “E,” “F,” and “G.”

C. The drive-thru restaurant shall be limited to a maximum of eight seats, including patio seating. Bollards shall be installed to protect the patio from vehicle intrusion.

D. All menu boards and drive-thru speakers be equipped with adjustable volume controls so that the sound is not audible at the closest property line businesses.

E. Deliveries to the business shall not occur between 10:01 P.M. and 6:00 A.M.

F. All rooftop mechanical equipment be screened from view at ground level. The applicant shall submit a line of site analysis and revise the plans if necessary, through means such as increasing the height of the parapet as necessary, to comply with this condition.

G. The project shall comply with the recommended conditions of approval from Development Review Board Case No. 9251.

H. Effective Date. The action by the PEC in this matter shall be final, conclusive, and effective five (5) city business days after the giving of official notice of approval, as provided in LMC Section 9403.2, unless within that period of time an appeal in writing is filed with the City Clerk by the applicant or by any person who protested the application as a matter of record, or by any person who, received or was entitled to receive the written notice specified in LMC Section 9422. The filing of such an appeal within such time limit or the request for review by a City

Council Member of such a decision within such time limit shall stay the effective date of the order granted until the City Council has acted on the appeal or other approval review.

I. Acceptance. By orally accepting the terms and provisions of this conditional use permit during the public hearing and as further recorded as entered into the minutes of these proceedings, the permittee thereby acknowledges all of the conditions imposed and accepts this permit subject to those conditions with a full awareness that the violation of any of the conditions could subject permittee to further hearings before the Commission on the issue of possible modification and/or revocation. Furthermore, the property owner is required to sign a written “Declaration of Restrictions” stating that they have read, understand, and agree to implement the conditions of approval.

J. Modification/Revocation. This CUP may be modified or revoked by the City Council or the PEC at a public hearing, should they determine that the proposed uses or conditions under which it is being operated or maintained is detrimental to the public health, welfare and/or materially injurious to property or improvements in the vicinity of the subject property and/or if the property is operated or maintained so as to constitute a public nuisance.

K. Indemnification. The applicant agrees to indemnify, hold harmless and defend the City, its officers, agents, and employees at the applicant’s expense from any and all liability or claims that may be brought against the City arising from its approval of this permit, and/or arising out of the operation of any business, land use or activity on the subject property, save and except where such are caused by the City’s active negligence.

L. Compliance. The applicant shall comply with all federal, state, and local laws. Material violation of any of those laws in connection with the construction and/or operation of any activity or land use on the subject property may be cause to justify modifications or revocation of this CUP by the PEC or the City Council at a public hearing.

SECTION 5. A copy of this Resolution shall be delivered to the applicant.

ADOPTED AND APPROVED this 1st day of May, 2025, by the Planning and Environment Commission of the City of Lakewood voting as follows:

AYES:	COMMISSIONERS:
NOES:	COMMISSIONERS:
ABSENT:	COMMISSIONERS:
ABSTAIN:	COMMISSIONERS:

Vicki Stuckey, Chairperson

ATTEST:

Aldo Cervantes, Secretary

DIVIDER SHEET



**CITY OF LAKEWOOD
PLANNING AND ENVIRONMENT COMMISSION
REVIEW REPORT**

Meeting Date: May 1, 2025

From: Paul Kuykendall, AICP, Senior Planner 
Aldo Cervantes, Director of Community Development 

Subject: Conditional Use Permit No. 1013, One-Year Review

Location: 5227 Lakewood Boulevard

Proposed Use: Second Hand Store

Applicant: Savers

SUMMARY

On May 4, 2023, the Planning and Environment Commission (PEC) adopted Resolution No. 4-2023 approving Conditional Use Permit No. 1013 for Savers secondhand store located at 5227 Lakewood Boulevard (see attached Resolution). Condition J of Section 4 of the Resolution requires a six-month and a one-year review from the date of approval of the Resolution. This is the one-year review.

Staff inspected the location on April 24, 2025 found that the business to be in compliance with Resolution No. 4-2023.

Staff recommends that the PEC receive and file this report. No further action is required for monitoring this location.